

## FORWARD

THE CONSTRUCTION STANDARDS AND DRAWINGS HAVE BEEN PREPARED TO AID ENGINEERS AND DEVELOPERS IN THE PREPARATION OF DEVELOPMENT PLANS AND ENGINEERING DESIGN AND TO INFORM INTERESTED PERSONS OF THE PROCEDURES AND STANDARDS FOR THE VILLAGE OF JOHNSTOWN, OHIO. IT IS ALSO INTENDED TO BE USED DURING RECONSTRUCTION OR REPLACEMENT OF EXISTING FACILITIES OR ANY CONSTRUCTION WITHIN THE VILLAGE RIGHT-OF-WAY. THE RULES, STANDARDS, SPECIFICATIONS, CRITERIA, ETC. ARE TO SUPPLEMENT ANY APPLICABLE ZONING REGULATIONS AND THE VILLAGE OF JOHNSTOWN SUBDIVISION REGULATIONS.

IT IS NOT THE INTENT OF THE CONSTRUCTION STANDARDS AND DRAWINGS TO TAKE AWAY FROM THE DESIGNING ENGINEER ANY RESPONSIBILITY FOR THE TECHNICAL ADEQUACY OF THIS DESIGN OR FREEDOM TO USE HIS ENGINEERING JUDGMENT AND DISCRETION. IT IS RECOGNIZED THAT MATTERS OF ENGINEERING DESIGN CANNOT BE SET OUT IN WRITING TO COVER ALL SITUATIONS; HOWEVER THE CONSTRUCTION STANDARDS AND DRAWINGS AS SET OUT HEREIN REPRESENT GOOD ENGINEERING PRACTICE. ANY DESIGN METHODS OR CRITERIA DIFFERENT THAN THAT LISTED WILL RECEIVE CONSIDERATION FOR APPROVAL. PROVIDED THE PROPOSED VARIANCES AND THE REASONS FOR THEIR USE ARE SUBMITTED TO THE VILLAGE FOR THEIR APPROVAL.

THE VILLAGE, AT ANY TIME DURING THE DESIGN OR CONSTRUCTION, SHALL HAVE THE AUTHORITY TO MODIFY ANY ENGINEERING OR CONSTRUCTION DETAIL, WHENEVER REQUIRED FOR THE PROTECTION OF THE PUBLIC INTEREST.

## UTILITY CONTACTS

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# FOREWORD

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## STREET FUNCTIONAL CLASSIFICATIONS

THE VILLAGE ENGINEER WILL PROVIDE THE CLASSIFICATION OF ALL STREETS PRIOR TO DESIGN AND CONSTRUCTION. THE CLASSIFICATIONS ARE AS FOLLOWS:

### A. ARTERIAL STREET

MEANS A STREET CONNECTING JOHNSTOWN WITH OUTSIDE ACTIVITY CENTERS AND/OR SERVING AS THE PRIMARY ROUTES THROUGH AND WITHIN THE VILLAGE. US 62 IS A PRIME EXAMPLE. ARTERIAL STREETS CARRY THE LARGEST VOLUME OF TRAFFIC USUALLY ON A CONTINUOUS ROUTE. SERVICE TO THE ADJACENT LAND IS SUBORDINATE TO THE PROVISION OF TRAVEL SERVICE ON ARTERIAL STREETS.

### B. COLLECTOR STREET (MAJOR)

MEANS A THOROUGHFARE WHICH CARRIES VEHICULAR TRAFFIC FROM LOCAL STREETS TO ARTERIAL STREETS, AND IS DESIGNED TO ACCOMMODATE 2,000 - 10,000 VEHICLES PER DAY ADT.

### C. COLLECTOR STREET (MINOR)

MEANS A THOROUGHFARE WHICH PRIMARILY CARRIES VEHICULAR TRAFFIC FROM LOCAL STREETS TO MAJOR COLLECTOR AND ARTERIAL STREETS, AND IS DESIGNED TO ACCOMMODATE 500-2,000 VEHICLES PER DAY ADT.

### D. CUL-DE-SAC

MEANS A SHORT LOCAL STREET HAVING BUT ONE END OPEN FOR MOTOR TRAFFIC AND THE OTHER END TERMINATED BY A VEHICULAR TURN-AROUND OR BACK-AROUND.

### E. LOCAL STREET

MEANS A STREET ON WHICH THE MAJORITY OF THE TRAFFIC ORIGINATES OR TERMINATES IN THE ABUTTING PROPERTIES. THESE STREETS ARE DESIGNED TO ACCOMMODATE UP TO 500 VEHICLES PER DAY ADT AT LOW SPEEDS.

### F. INDUSTRIAL STREET

MEANS A STREET ON WHICH MORE THAN TWENTY-FIVE PERCENT (25%) OF THE TRAFFIC IS COMPRISED OF TRUCKS, OR WHERE MORE THAN FIFTY PERCENT (50%) OF THE ABUTTING PROPERTY IS EITHER OCCUPIED BY INDUSTRIAL USES.

#### DESIRED MINIMUM STANDARDS

STREET FUNCTIONAL CLASSIFICATION	RIGHT-OF-WAY WIDTH	BACK-TO-BACK CURB
	(L.F.)	(L.F.)
ARTERIAL	100*	64*
MAJOR COLLECTOR	70	41
MINOR COLLECTOR	70	36
CUL-DE-SAC	60	30
LOCAL STREET	60	30
INDUSTRIAL STREET	60	41

\* SEE DESIGN CRITERIA

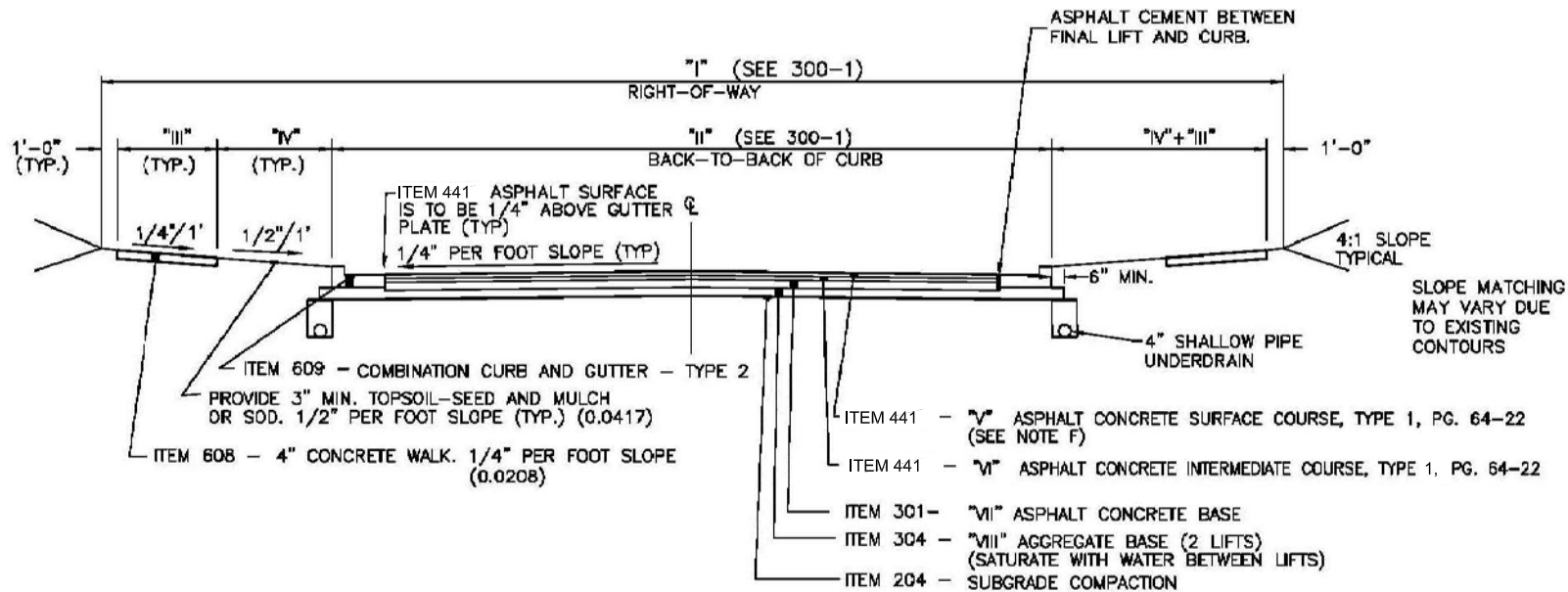


## STREET CLASSIFICATIONS AND RECOMMENDED STANDARD STREET DIMENSIONS

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#### MINIMUM STANDARDS

	ITEM	DESCRIPTION	ARTERIAL	INDUSTRIAL	COLLECTOR (MAJOR)	COLLECTOR (MINOR)	LOCAL	CUL-DE-SAC
STANDARD DIMENSIONS	I	RIGHT-OF-WAY	100'	60'	70'	70'	60'	60'
	II	PAVEMENT WIDTH*	**	41'	41'	36'	30'	30'
	III	SIDEWALK WIDTH	5'	5'	5'	5'	5'	5'
	IV	CURB LAWN WIDTH	**	5'	7'	4'	4'	4'
AGGREGATE BASE	V	ITEM 441, SURFACE	1 - 1/4"	1 - 1/4"	1 - 1/4"	1 - 1/4"	1 - 1/4"	1 - 1/4"
	VI	ITEM 441, INTERMEDIATE	1 - 3/4"	1 - 3/4"	1 - 3/4"	1 - 3/4"	1 - 3/4"	1 - 3/4"
	VII	ITEM 301	7"	4"	4"	3"	3"	3"
	VIII	ITEM 304	6"	9"	9"	9"	6"	6"
ASPHALT BASE	V	ITEM 441, SURFACE	1 - 1/4"	1 - 1/4"	1 - 1/4"	1 - 1/4"	1 - 1/4"	1 - 1/4"
	VI	ITEM 441, INTERMEDIATE	1 - 3/4"	1 - 3/4"	1 - 3/4"	1 - 3/4"	1 - 3/4"	1 - 3/4"
	VII	ITEM 301	9"	7"	7"	6"	5"	5"
	VIII	ITEM 304	2 LIFTS	2 LIFTS	2 LIFTS	2 LIFTS	2 LIFTS	2 LIFTS

#### NOTES:

- ALL WORK TO CONFORM TO ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS LATEST REVISION UNLESS OTHERWISE SPECIFIED.
- ITEM 407 TACK COAT SHALL BE REQUIRED WHEN 5 DAYS HAVE ELAPSED BETWEEN BITUMINOUS PAVEMENT LIFTS UNLESS OTHERWISE SPECIFIED BY THE ENGINEER. APPLICATION RATE IS 0.10 GALLON PER SQUARE YARD.
- ALL BUTT JOINTS SHALL BE SEALED WITH PG 64-22 WITHIN 24 HOURS AFTER PLACEMENT OF ITEM 441.
- ASPHALT REJUVENATING AGENT (COLUMBUS CMS SPECIAL PROVISION 1540) SHALL BE APPLIED TO THE SURFACE COURSE OF ITEM 441 WITHIN 1 WEEK OF IT BEING PLACED UNLESS OTHERWISE SPECIFIED BY THE ENGINEER.
- STANDARD DIMENSIONS FOR PAVEMENT WIDTH (BACK-TO-BACK OF CURB) AND APRON WIDTH ASSUME PARKING ON BOTH SIDES OF THE STREET.
- CONCRETE PAVEMENT WILL BE CONSIDERED UPON SUBMITTAL OF TYPICAL SECTION DESIGN AND APPROVED BY THE PLANNING AND ZONING COMMISSION.

\* BACK-TO-BACK OF CURB

\*\* PAVEMENT WIDTH AND CURB LAWN WIDTH ON ARTERIAL STREETS TO BE DETERMINED ON A CASE-BY-CASE BASIS BY THE PLANNING AND ZONING COMMISSION.



## TYPICAL SECTIONS AND ASPHALT PAVEMENT COMPOSITION

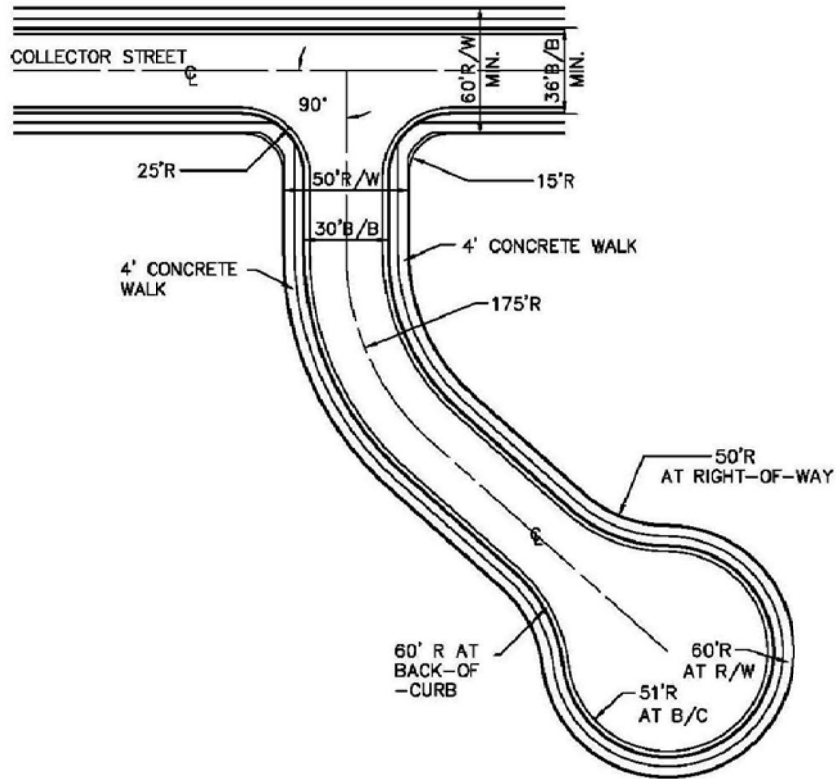
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## TYPICAL STREET AND CUL-DE-SAC PLAN

## STREET DESIGN STANDARDS



	* 25 mph LOCAL (THRU STREETS)	* 35 mph COLLECTOR
MINIMUM CENTERLINE GRADES	.50%	.50%
MAXIMUM CENTERLINE GRADES	6%	6%
MINIMUM LENGTH OF VERTICAL CURVE (SEE NOTE C).	25FT.	50FT.
MINIMUM CENTERLINE RADIUS	250FT.	400FT.
MINIMUM LENGTH TANGENT BETWEEN CURVES	50FT.	50FT.
MINIMUM BACK-OF-CURB RADIUS	25FT.	25FT.
MINIMUM HORIZONTAL VISIBILITY	150FT.	250FT.
MINIMUM STOPPING SIGHT DISTANCE (MEASURED FROM 3.5' EYE-LEVEL TO 6" OBJECT HEIGHT)	150FT.	250FT.
MAXIMUM CENTERLINE GRADE WITHIN 100' OF AN INTERSECTION	3%	3%
RIGHT-OF-WAY WIDTH	50FT.	60FT.
MINIMUM PAVEMENT WIDTH BACK-TO-BACK OF CURB	30FT.	36FT.

### NOTES

A. THESE ARE MINIMUM DESIGN STANDARDS AND MAY BE REQUIRED TO BE INCREASED TO COMPLY WITH THE VILLAGE OFFICIAL THOROUGHFARE PLAN.

B. THE MAXIMUM LENGTH FOR CUL-DE-SAC STREET SHALL BE 600' CENTER-OF-STREET TO CENTER OF CUL-DE-SAC UNLESS AUTHORIZED BY CITY VILLAGE PLANNING AND ZONING COMMISSION.

C. MINIMUM LENGTH OF VERTICAL CURVE CAN BE REDUCED OR ELIMINATED TO ALLOW FOR PROPER DRAINAGE, WITH APPROVAL OF THE VILLAGE.

\* THESE ARE DESIGN SPEEDS, NOT NECESSARILY POSTED SPEEDS.

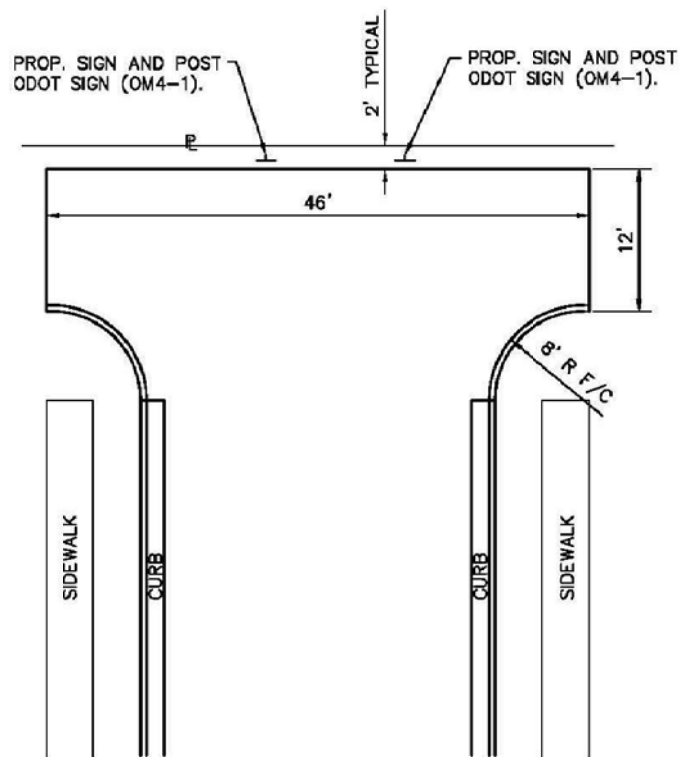


## STREET DESIGN STANDARDS

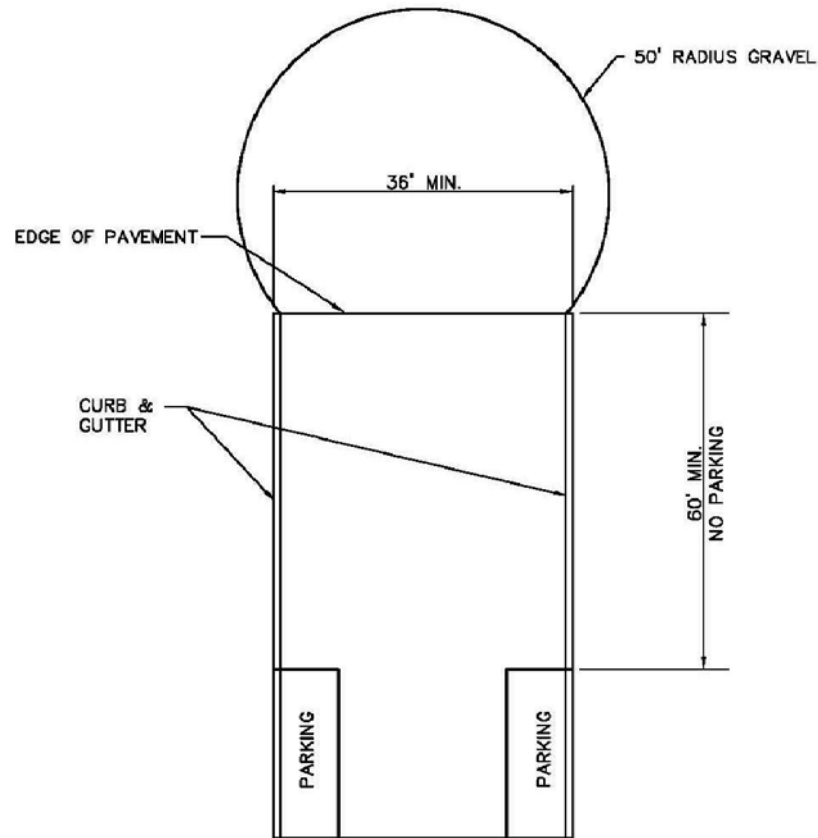
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**TEMPORARY HAMMER HEAD**



**TEMPORARY DEAD-END STREETS**

**NOTES**

**A.** TEMPORARY DEAD END STREETS ARE ONLY PERMITTED IN SITUATIONS WHERE EXTENSION OF ROADWAY IS PLANNED IN A PHASED DEVELOPMENT AND THE ADJOINING PHASE IS UNDER CONSTRUCTION OR BONDED FOR DEVELOPMENT. IF DEVELOPMENT OF THE NEXT PHASE IS NOT IMMEDIATE A GRAVEL CUL-DE-SAC IS REQUIRED TO BE INSTALLED AND MAINTAINED.



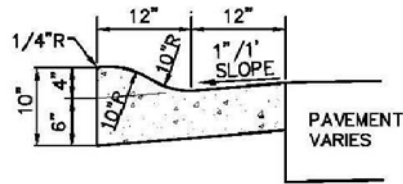
# TEMPORARY DEAD-END STREET

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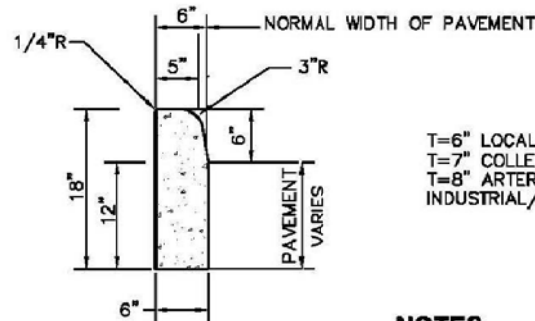
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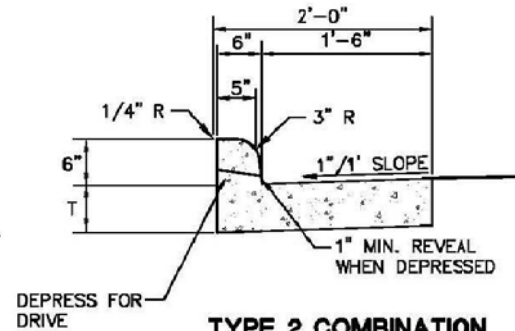


**TYPE 1  
COMBINATION ROLL CURB AND GUTTER**



**TYPE 6  
BARRIER CURB**

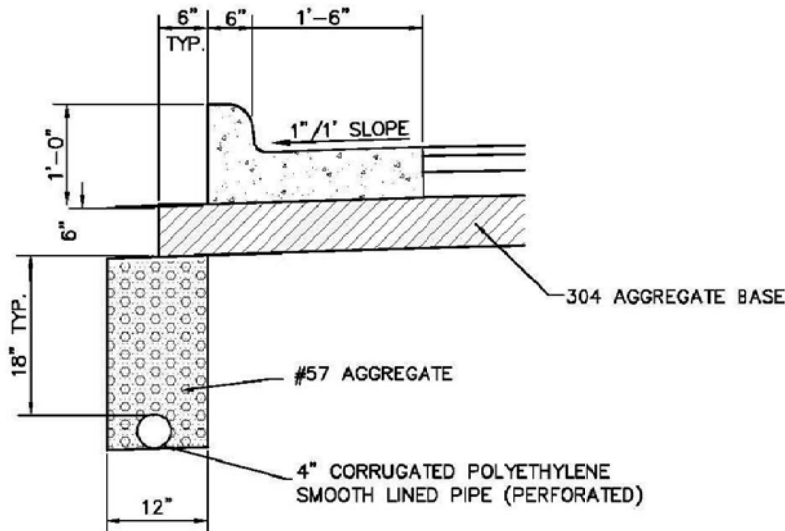
T=6\" LOCAL  
T=7\" COLLECTOR  
T=8\" ARTERIAL/  
INDUSTRIAL/COMMERCIAL



**TYPE 2 COMBINATION  
CURB AND GUTTER**

## NOTES

- A. CONCRETE WORK SHALL MEET THE REQUIREMENTS SET FORTH IN ODOT ITEM 609 CURBING.
- B. CURBING SHALL HAVE CONTRACTION JOINTS EVERY 10'.
- C. MINIMUM OF 6\" OF ODOT 304 SHALL BE PLACED UNDER CURBING.
- D. CURBING SHALL BE BACKFILLED IMMEDIATELY AFTER FORMS ARE REMOVED OR AS SOON AS PRACTICAL WHEN SLIP FORMING PRIOR TO OTHER CONSTRUCTION OPERATIONS.
- E. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.
- F. APPLY CLEAR CURING COMPOUND ON ALL SURFACES INCLUDING BACK IMMEDIATELY AFTER FINISHING SURFACES. ANY OTHER METHOD OR TYPE OF CURING COMPOUND MUST BE PREAPPROVED.
- G. CONCRETE SHALL BE ODOT CLASS C WITH FIBERMESH (4000 PSI, 600LB/CY CEMENT). PROPORTIONING OPTIONS 1 AND 2 NOT ALLOWED.
- H. CONCRETE SHALL CONTAIN 6%  $\pm$  1% OF TOTAL AIR.
- I. TYPE 6 CURBS ARE FOR USE AROUND MEDIAN SECTION.
- J. UNDERDRAIN MUST BE INSTALLED PRIOR TO CURB INSTALLATION.
- K. MINIMUM FLOW LINE SLOPE OF PERFORATED PIPE IS 0.003 FT/FT TO OUTLET.
- L. MACHINE CUTTING THE BACK OF CURB MAY BE DONE. APPROVAL FROM VILLAGE ENGINEER PRIOR TO CUTTING IS REQUIRED.



**4\" SHALLOW PIPE UNDERDRAIN DETAIL**  
TO BE USED IN NEW DEVELOPMENT WHEN  
WARRANTED BY GROUND WATER CONDITIONS.



# CONCRETE CURB DETAILS

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**NOTES**

L. THE CONTRACTOR SHALL ADD THE FOLLOWING BRANDS TO CURB AND GUTTER, THREE INCHES HIGH. ALL BRANDS SHALL BE MADE WHEN THE CONCRETE IS WET AND THE PROPOSED BRANDS SHALL BE REVIEWED BY THE VILLAGE PRIOR TO THE BRANDING.

- 1. "W" FOR ALL WATER SERVICES, "SHORT" AND "LONG".
- 2. "WV" AT ALL WATER VALVE LOCATIONS.
- 3. "WW" AT LOCATIONS WHERE WATER MAIN CROSSES BENEATH THE CURB.
- 4. "LS" AT ALL LOCATIONS WHERE "LONG" SANITARY SERVICE CONNECTION CROSSES BENEATH THE CURB.
- 5. "SS" AT ALL "SHORT" SANITARY SERVICE CONNECTIONS UNLESS LOCATED BEHIND THE RESIDENCE.
- 6. "X" AT ALL LOCATIONS WHERE OTHER UTILITIES-ELECTRICAL SERVICE, GAS, ETC.- CROSS BENEATH THE CURB.

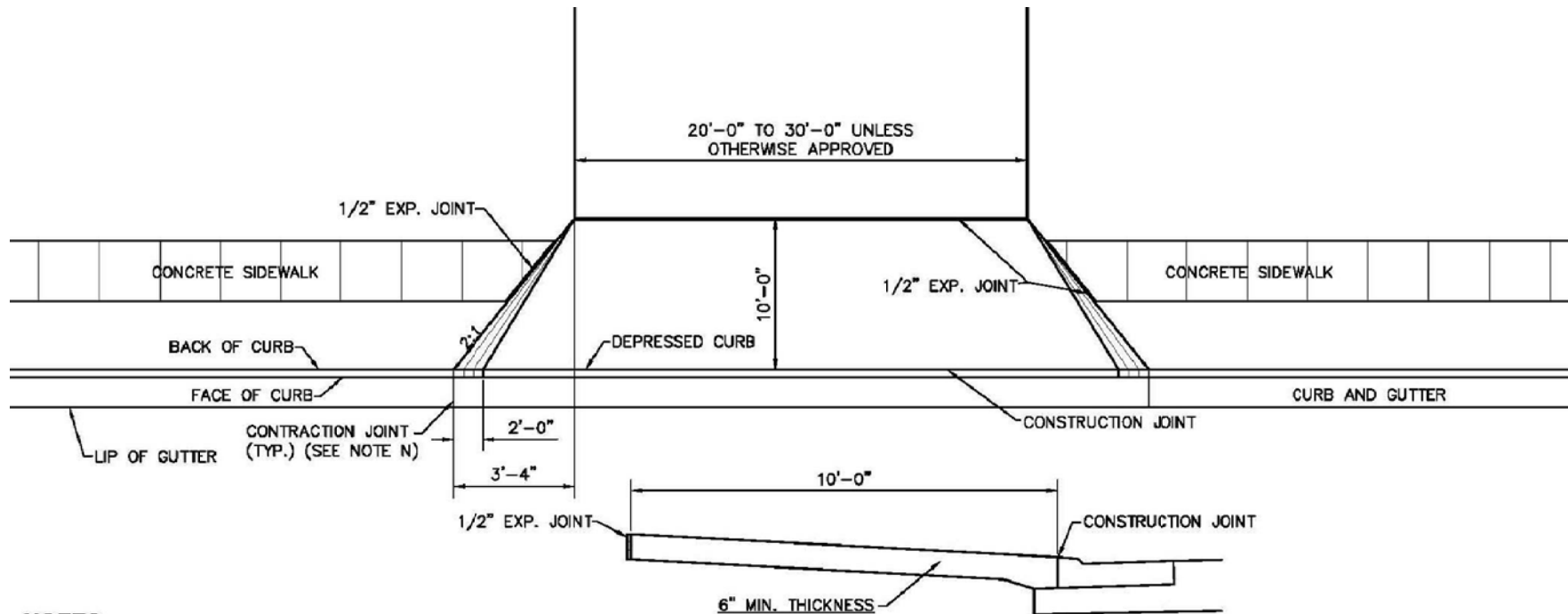


CONCRETE CURB DETAILS

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## NOTES

**A.** DRIVE APPROACHES SHALL MEET THE REQUIREMENTS OF ODOT ITEM 452 AND 499 CAST IN PLACE CONCRETE.

**B.** DRIVE APPROACHES SHALL NOT BE POURED MONOLITHICALLY WITH CURB.

**C.** MAXIMUM JOINT SPACING SHALL BE 10' LONGITUDINALLY AND TRANSVERSELY WITH JOINTS AT TAPERS.

**D.** EXPANSION MATERIAL SHALL BE 1/2" PREMOLDED.

**E.** COMPACTED AGGREGATE SHALL BE PLACED UNDER DRIVE APPROACHES IF DETERMINED NECESSARY BY THE CITY.

**F.** PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.

**G.** WHERE CURB AND GUTTER HAS NOT BEEN PROPERLY DROPPED AT DRIVE APPROACHES, THE CURB SHALL BE ENTIRELY REMOVED AND REPLACED TO THE NEAREST CONTRACTION JOINT BEYOND THE DRIVEWAY AND REPLACED BY EITHER THE CONTRACTOR OR THE OWNER. NO SAWCUTTING OF THE CURB IS PERMITTED.

**H.** WHERE ASPHALTIC CONCRETE PAVEMENT IS DISTURBED, THE ASPHALT SHALL BE REPLACED AS DIRECTED BY THE CITY INSPECTOR.

**I.** JOINTS SHALL BE CLEANED AND EDGED BY A 1/4" RADIUS EDGER. LONGITUDINAL JOINTS SHALL BE AS DIRECTED BY THE VILLAGE. EXPANSION JOINTS SHALL BE OF SUCH DIMENSIONS AS SHOWN ON STANDARD DRAWINGS FOR CONSTRUCTION JOINTS.

**J.** MINIMUM WIDTH FOR ONE-WAY TRAFFIC IS 20'-0". MINIMUM WIDTH FOR TWO-WAY TRAFFIC IS 25'-0". MAXIMUM WIDTH IS 30'-0" UNLESS OTHERWISE APPROVED BY PUBLIC WORKS DIRECTOR.

**K.** THIS STANDARD DRAWING IS FOR GUIDELINE PURPOSES. EACH INDIVIDUAL DRIVE WILL NEED TO BE FIELD DESIGNED AND APPROVED BY THE CITY INSPECTOR PRIOR TO PLACING CONCRETE.

**L.** CONCRETE SHALL BE ODOT CLASS C. (4000 PSI, 600 LB/CY CEMENT. PROPORTIONING OPTIONS 1, 2, AND 3 NOT ALLOWED.

**M.** CONCRETE SHALL CONTAIN 6%  $\pm$  1% OF TOTAL AIR.

**N.** IF CURB IS REMOVED DURING DRIVEWAY CONSTRUCTION IT MUST BE REPLACED TO AN EXISTING CONTRACTION JOINT AND 1/2" EXPANSION MATERIAL IS REQUIRED THE FULL DEPTH OF WHERE THE NEW CURB MEETS THE EXISTING. NO SAWCUTTING OF CURB BETWEEN JOINTS IS PERMITTED.

**O.** A RIGHT-OF-WAY PERMIT IS REQUIRED FOR ANY AND ALL WORK IN THE RIGHT-OF-WAY INCLUDING A \$10 DRIVEWAY FEE, IF APPLICABLE, THROUGH THE ENGINEERING DEPARTMENT.

**P.** AS PER VILLAGE ORDINANCE A-2588 DRIVEWAY CURBING IS THE PROPERTY OWNER'S RESPONSIBILITY TO MAINTAIN.

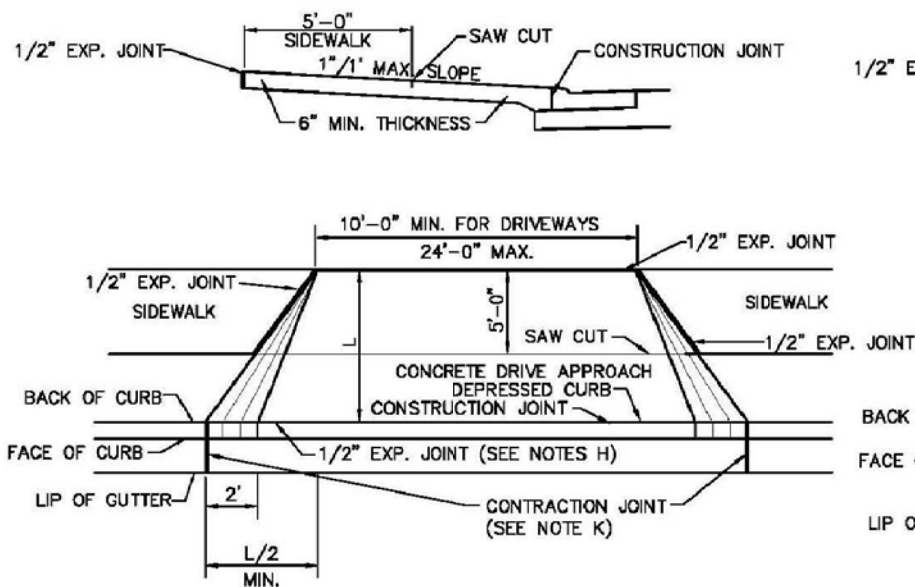


# COMMERCIAL AND INDUSTRIAL DRIVE APPROACH

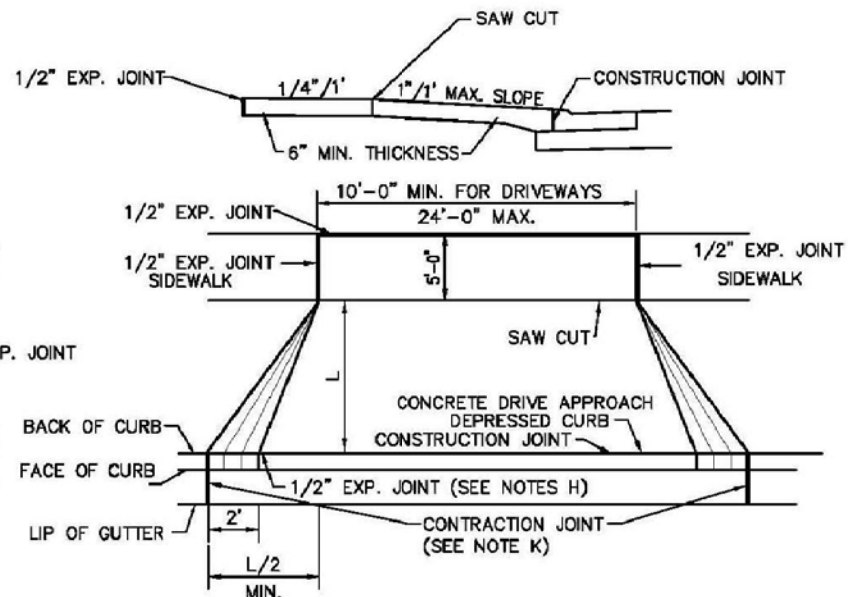
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**FOR TREE LAWNS OF  
LESS THAN 6'-0"**



**FOR TREE LAWNS OF  
6'-0" OR MORE**

## NOTES

- A. DRIVE APPROACHES SHALL MEET THE REQUIREMENTS OF ODOT ITEM 452 AND 499 CAST-IN-PLACE CONCRETE.
- B. MAXIMUM JOINT SPACING SHALL BE 10' LONGITUDINALLY, TRANSVERSELY AND AT TAPERS.
- C. EXPANSION MATERIAL SHALL BE 1/2" PREMOLDED.
- D. 3" OF GRAVEL SHALL BE PLACED UNDER DRIVE APPROACHES IF DETERMINED NECESSARY BY THE CITY.
- E. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.

F. WHERE CURB AND GUTTER HAS NOT BEEN PROPERLY DROPPED AT DRIVE APPROACHES, THE CURB SHALL BE ENTIRELY REMOVED AND REPLACED TO THE NEAREST CONTRACTION JOINT BEYOND THE DRIVEWAY AND REPLACED BY EITHER THE CONTRACTOR OR THE OWNER. NO SAWCUTTING OF THE CURB IS PERMITTED.

G. JOINTS SHALL BE CLEANED AND EDGED BY A 1/4" RADIUS EDGER. LONGITUDINAL JOINTS SHALL BE AS DIRECTED BY THE CITY. EXPANSION JOINTS SHALL BE OF SUCH DIMENSIONS AS SHOWN ON STANDARD DRAWINGS FOR CONSTRUCTION JOINTS.

H. EXPANSION JOINT LOCATION MAY BE ALTERED WITH CITY APPROVAL.

I. CONCRETE SHALL BE ODOT CLASS C (4000 PSI, 600 LB/CY) CEMENT. PROPORTIONING OPTIONS 1, 2, AND 3 NOT ALLOWED.

J. CONCRETE SHALL CONTAIN 6% ± 1% OF TOTAL AIR.

K. IF CURB IS REMOVED AND REPLACED DURING DRIVEWAY CONSTRUCTION, JOINTS BETWEEN EXISTING AND NEW CURB ARE TO CONTAIN 1/2" EXPANSION MATERIAL.

L. A RIGHT-OF-WAY PERMIT IS REQUIRED FOR ANY AND ALL WORK IN THE RIGHT-OF-WAY INCLUDING A \$10 DRIVEWAY FEE, IF APPLICABLE, THROUGH THE ENGINEERING DEPARTMENT.



# RESIDENTAL DRIVE APPROACH

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**DRIVEWAY REPLACEMENT**

CONTRACTOR SHALL INSTALL APPROACHES PER THE DETAIL SHOWN IN THE PLANS. APPROACH WILL CONSIST OF THE DRIVEWAY FROM THE BACK OF CURB TO THE BACK OF WALK AND INCLUDING THE AREA OF THE WALK. ALL DRIVEWAYS SHALL HAVE WIRE MESH OR FIBER ADDITIVE REINFORCEMENT.

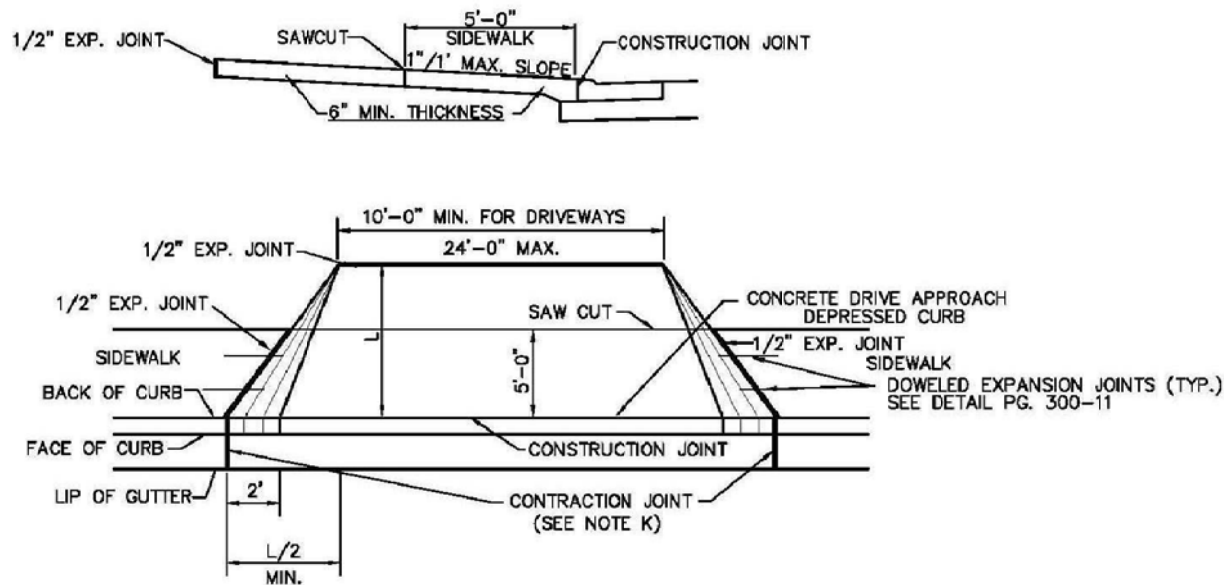
IF DAMAGE TO THE DRIVEWAY OCCURS BEHIND THE WALK, IT IS THE CONTRACTOR'S RESPONSIBILITY TO REPAIR. THE CONTRACTOR SHALL SAWCUT THE DRIVE, EXCAVATE THE EXISTING MATERIAL AND REPLACE THE DRIVE IN KIND. WHEN A TRENCH ACROSS A DRIVEWAY IS LOCATED WITHIN OR NEAR THE RIGHT-OF-WAY, ASPHALT DRIVES SHALL BE REPLACED FROM THE FURTHEST EDGE OF THE TRENCH TO THE EDGE OF THE ROAD PAVEMENT OR TO THE CONCRETE DRIVE APRON, IF ONE EXISTS. CONCRETE DRIVES SHALL BE REPLACED BETWEEN NEAREST ADJACENT JOINTS IF JOINTS ARE WITHIN 4 FEET OF TRENCH EDGE. ALL DRIVEWAYS DISTURBED OR DAMAGED DURING WORK UNDER THIS CONTRACT SHALL BE RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR UNLESS OTHERWISE SPECIFIED.



**RESIDENTAL DRIVE APPROACH**

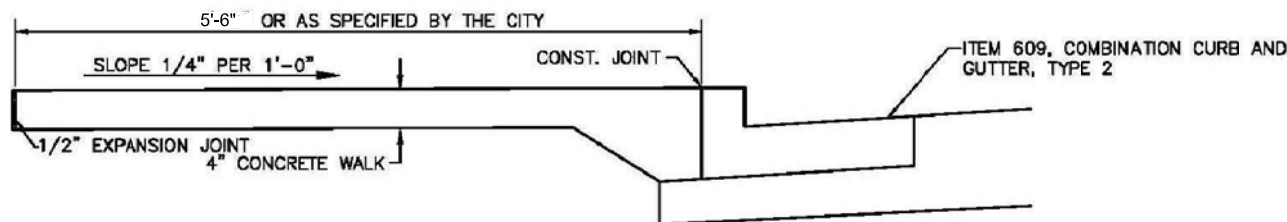
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**DRIVE APRON WITH  
NO CURB LAWN**

FOR DRIVEWAY NOTES SEE PAGE 300-7



**CONCRETE SIDEWALK ABUTTING TYPE 2 CURB DETAIL**



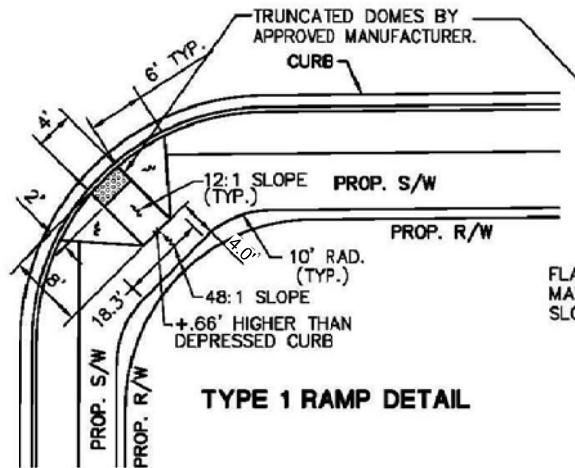
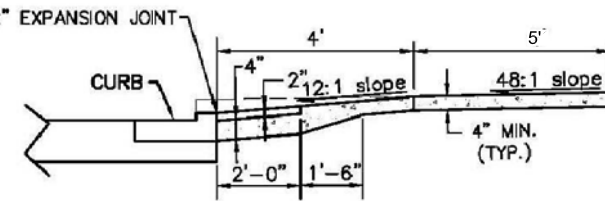
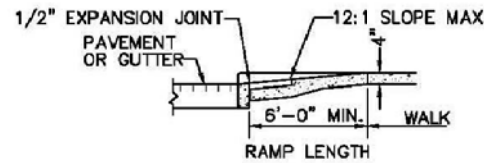
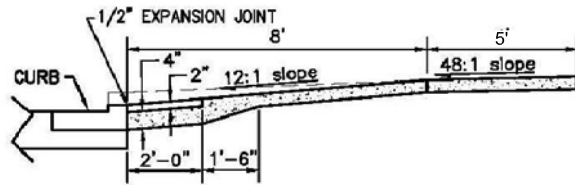
**RESIDENTIAL DRIVE APPROACH AND CONCRETE  
SIDEWALK DETAIL WITH NO CURB LAWN**

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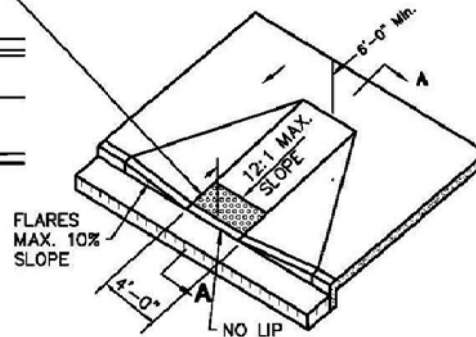
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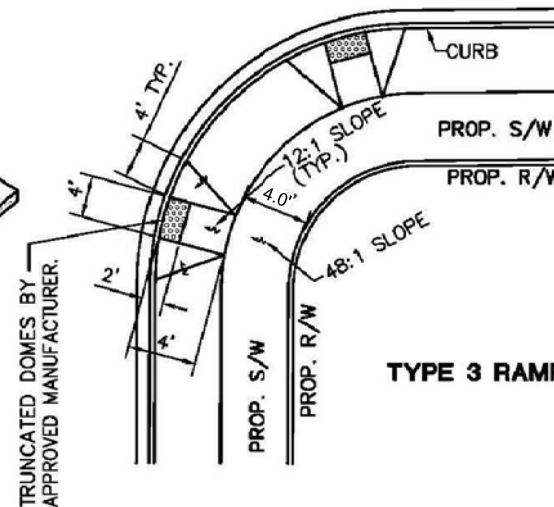




**TYPE 1 RAMP DETAIL**



**TYPE 2 RAMP DETAIL**



**TYPE 3 RAMP DETAIL**

## NOTES

- A. VILLAGE TO SPECIFY TYPE 1, 2, OR 3 CURB RAMP.
- B. ANY COMBINATION OF SIDE SLOPES ON OPPOSITE SIDES OF A RAMP MAY BE USED TO BEST FIT THE SITE CONDITIONS.
- C. THE MINIMUM RAMP LENGTH IS 6' FROM BACK OF A 6" CURB AND MAY BE INCREASED WHERE FEASIBLE TO OBTAIN A FLATTER RAMP SLOPE OR TO BETTER BLEND WITH THE WALK CONFIGURATION.
- D. WALK THICKNESS IN THE RAMP SLOPES SHALL BE 4" MINIMUM OR THICKER AS NECESSARY TO MATCH ADJACENT WALK THICKNESS.
- E. CURB RAMP SLOPES SHALL MEET AND BE FINISHED TO AMERICANS WITH DISABILITIES ACT (A.D.A.) STANDARDS.
- F. TEXTURE OF CONCRETE SURFACE SHALL BE OBTAINED BY COURSE BROOMING TRAVERSE TO THE RAMP SLOPES AND SHALL BE ROUGHER THAN ADJACENT WALK.
- G. CURB RAMP SLOPES SHALL MEET THE REQUIREMENTS OF ODOT ITEM 608 UNLESS OTHERWISE SPECIFIED WITHIN.
- H. CONCRETE SHALL BE ODOT CLASS C (4000 PSI, 600 LB/CY CEMENT), PROPORTIONING OPTIONS 1, 2, AND 3 NOT ALLOWED.
- I. CONCRETE SHALL CONTAIN 6%  $\pm$  1% OF TOTAL AIR.
- J. FOR RECONSTRUCTION JOBS, THE CURB RAMP SLOPES WILL HAVE TO BE ADDRESSED BASED ON THE EXISTING CONDITIONS.

## K. TRUNCATED DOME SPECIFICATIONS:

INSTALL DETECTABLE WARNINGS (TRUNCATED DOMES) FOR A DISTANCE OF 24" FROM THE BACK OF CURB FOR THE ENTIRE WIDTH OF THE RAMP OPENING WHERE IT IS FLUSH WITH THE PAVEMENT.

—ACCEPTABLE MANUFACTURER AND PRODUCT IS: EAST JORDAN IRON WORKS #7005 OR EQUIVALENT, UNLESS APPROVED BY THE ENGINEER IN WRITING. COLOR SHALL BE RUSTIC RED #30.

J. HANDICAP RAMP SLOPES AND SIDEWALKS SHALL BE CONSTRUCTED IN FULL COMPLIANCE WITH THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990, INCLUDING ALL SUPPLEMENTS AND IN ACCORDANCE WITH THE CITY OF COLUMBUS STANDARD DRAWINGS DATED JANUARY 1, 2007, INCLUDING ALL SUPPLEMENTS OR REPLACEMENT DRAWINGS ISSUED THERETO. SIDEWALKS SHALL BE CONSTRUCTED WITH A CROSS SLOPE NOT TO EXCEED 2.0%.

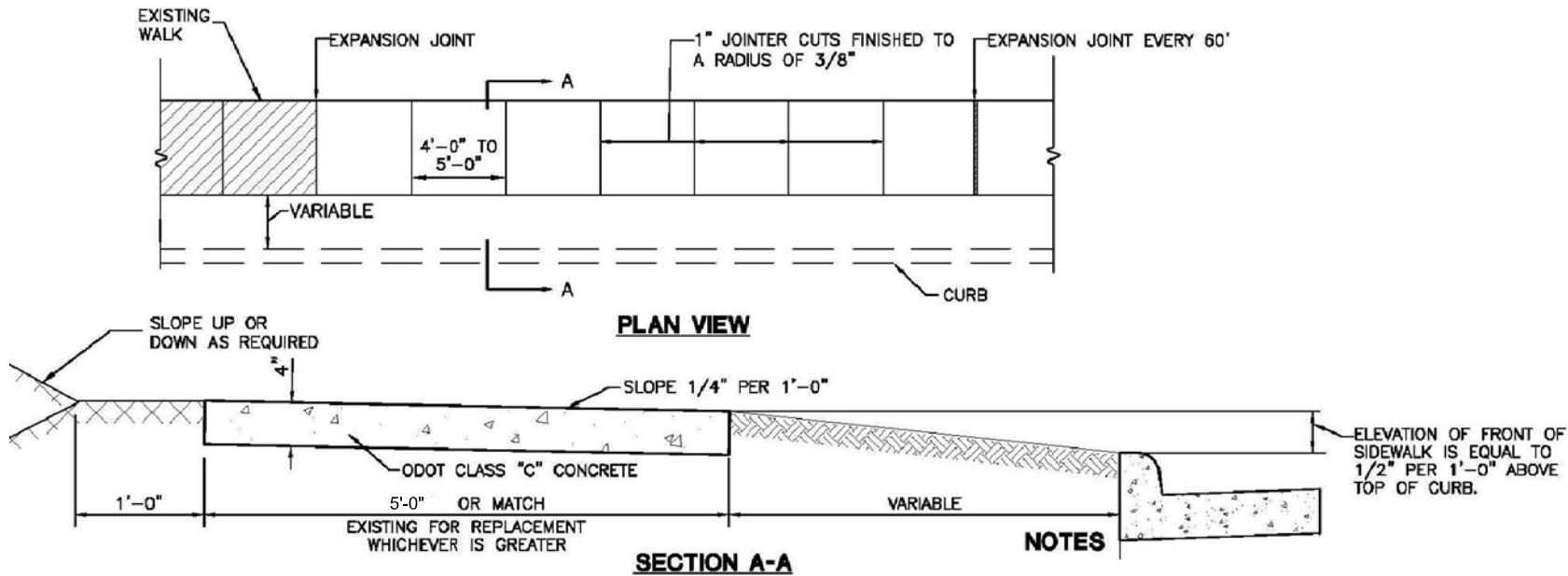


# CURB RAMPS

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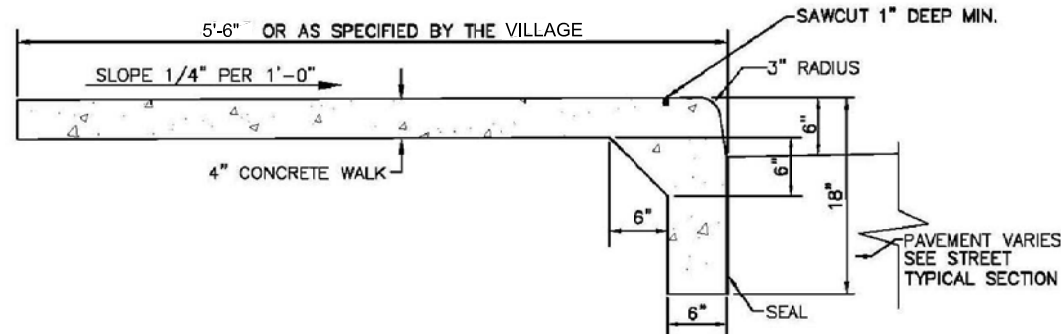
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#### NOTES

- A. WALK TO BE POURED ON UNDISTURBED EARTH OR COMPACTED GRANULAR BEDDING.
- B. PROVIDE BROOM FINISH TO ALL EXPOSED SURFACES.
- C. CONCRETE SHALL CONFORM TO ODOT ITEM 499 CONCRETE. CONCRETE WORK SHALL CONFORM TO ODOT ITEM 608, UNLESS OTHERWISE SPECIFIED WITHIN.
- D. PROVIDE EDGING AROUND ALL EXPOSED SURFACES.
- E. USE CLEAR CURING COMPOUND IMMEDIATELY AFTER FINISHING SURFACES. ANY OTHER METHOD OR TYPE OF CURING COMPOUND MUST BE PREAPPROVED.
- F. WHEN RENOVATING EXISTING STREETS, THE SIDEWALKS SHALL BE REPLACED TO CONFORM WITH THE CITY CONSTRUCTION STANDARDS AND DRAWINGS.
- G. CONCRETE SHALL BE ODOT CLASS C (4000 PSI, 600LB/CY CEMENT). PROPORTIONING OPTIONS 1 AND 2 NOT ALLOWED.
- H. CONCRETE SHALL CONTAIN  $6\% \pm 1\%$  OF TOTAL AIR.
- I. THE SIDEWALK SHALL HAVE 3-INCHES OF #304 AGGREGATE USED FOR BEDDING OF THE SIDEWALK. THE FINISHED EDGE OF ALL SIDEWALK SHALL BE TOOLED. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 608 - 4" CONCRETE WALK.

#### COMBINED CURB AND SIDEWALK DETAIL



# CONCRETE SIDEWALK DETAIL

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**EXAMPLE: 3/4 INCH TRIP HAZARD**



ADJOINING BLOCKS OR PORTIONS THEREOF WHOSE EDGES DIFFER VERTICALLY BY MORE THAN 3/4 INCH.



**EXAMPLE: DETERIORATION**



ANY SIDEWALK THAT IS DETERIORATED OR SHOWS SURFACE SPALLING, LEAVING IT VERY ROUGH, UNSAFE, OR WITH AGGREGATE PROTRUDING.

**EXAMPLE: ABRUPT SLOPE**



BLOCKS, OR PORTION OF BLOCKS, THAT CAUSE AN ABRUPT CHANGE OF 1 INCH PER FOOT (OR MORE) IN ANY DIRECTION OF THE SIDEWALK.



**EXAMPLE: PLATES, COVERS, ETC.**



METAL OR OTHER PLATES, COVERS, OR GRATINGS THAT ARE NOT FLUSH (3/4 INCH OR MORE VERTICAL DIFFERENCE) WITH THE ADJOINING SIDEWALK SURFACE, ARE STRUCTURALLY UNSAFE, OR CAUSE A NUISANCE DUE TO SLIPPERY SURFACES ETC.

**EXAMPLE: CRACKS**



ANY SIDEWALK BLOCK (BASED ON 20 SQ. FT.) HAVING A CRACK OR CRACKS IN IT OF AT LEAST 1/2 INCH WIDE WITH A MINIMUM OF 4 LINEAL FEET IN ONE BLOCK. (VARIOUS SIZE BLOCKS WILL BE EVALUATED PROPORTIONALLY.)



**PERMITS, INSPECTION, AND WORK RULES**

**A.** NO PERSON SHALL TEAR UP OR DIG INTO ANY PUBLIC RIGHT-OF-WAY OR STREET FOR THE PURPOSE OF CONSTRUCTING OR REPAIRING THE SIDEWALK, CURBING, OR GUTTERS THEREON OR FOR ANY OTHER PURPOSE, WITHOUT HAVING FIRST OBTAINED A PERMIT FROM THE SERVICE DEPARTMENT TO DO SO.

**B.** THE CONTRACTOR OR OWNER MUST CALL THE VILLAGE FOR AN INSPECTION AT LEAST THREE WORKING HOURS BEFORE POURING CONCRETE OR MAKING ANY REPAIRS TO THE SIDEWALK. THE CONTRACTOR OR HIS FOREMAN MUST BE PRESENT WHEN THE INSPECTOR ARRIVES. IF, DUE TO WEATHER CONDITIONS OR FOR SOME OTHER REASON IT NOT BE POSSIBLE TO HAVE A PERSON ON THE JOB, THE CONTRACTOR IS REQUIRED TO CALL AND CANCEL THE INSPECTION. ANY WALKS REPAIRED OR PLACED WITHOUT PRIOR INSPECTIONS WILL NOT BE ACCEPTED BY THE VILLAGE.

**C.** THE CONTRACTOR IS CAUTIONED AGAINST ORDERING CONCRETE BEFORE THE INSPECTION IS MADE DUE TO POSSIBLE CORRECTION OF FORMS OR GRADE.

**D.** THE CONTRACTOR SHALL PROVIDE PROTECTION AND TRAFFIC CONTROL BARRICADES, LIGHTS, SIGNS, AND OTHER DEVICES AS HEREIN SPECIFIED TO PROVIDE WARNING AND PROTECTION FOR VEHICULAR TRAFFIC, PEDESTRIANS, AND THE WORK DURING THE REMOVAL, CONSTRUCTION AND CURING OF SIDEWALK, CURB AND GUTTER, AND DRIVEWAY APRONS.

**E.** THE CONTRACTOR WILL BE RESPONSIBLE FOR AN IMMEDIATE REMOVAL AND CLEANUP OF ALL EXCAVATED MATERIAL. NO EXCAVATED MATERIAL SHALL BE STORED ON THE PAVEMENT.

**F.** ALL CONTRACTORS INSTALLING NEW CURB ARE CAUTIONED THAT IT IS THEIR RESPONSIBILITY TO REPAIR THE STREET PER VILLAGE SPECIFICATION BEFORE REMOVING YOUR BARRICADES.

**G.** GRINDING DOWN WILL ONLY BE APPROVED WITH THE FOLLOWING CONDITIONS: 1) CONCRETE SIDEWALK MUST REMAIN NO LESS THAN 2-1/2 INCHES IN THICKNESS FOR FINAL REPAIRS. 2) REPAIRED SIDEWALK MUST BE SMOOTH AND EVEN AND THE SLOPE SHALL BE NO GREATER THAN 12:1 SLOPE.

**H.** OWNER/CONTRACTOR SHALL REMOVE ALL FORMS, BACKFILL AND SEED OR SOD AREA WITHIN 72 HOURS OF POURING CONCRETE.

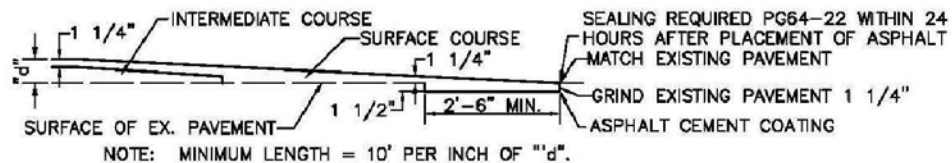


# GUIDELINES FOR REPLACEMENT OF SIDEWALK

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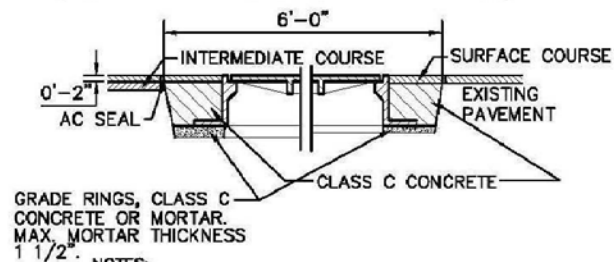
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**BUTT JOINT DETAIL**

## MANHOLES ADJUSTED TO GRADE FOR OVERLAYS



NOTES:  
MR. MANHOLE, MR. VALVE OR EQUIVALENT CASTING ADJUSTING EQUIPMENT MAY BE UTILIZED TO ADJUST CASTINGS TO PAVEMENT SURFACE. CONCRETE USED WITH THIS SYSTEM MUST BE DYED BLACK.

SEE STORM AND SANITARY MANHOLE DETAILS FOR ADDITIONAL DETAILS.



# ASPHALT OVERLAY AND MONUMENT

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## GENERAL

**A.** FAILURE TO COMPLY WITH THE CONSTRUCTION STANDARDS AND DRAWINGS AND DESIGN CRITERIA WILL REQUIRE REMOVAL AND REPLACEMENT IN ACCORDANCE WITH THESE STANDARDS.

**B.** ALL STREET CONSTRUCTION SHALL BE IN ACCORDANCE WITH ODOT SPECIFICATIONS, LATEST REVISION.

**C.** CONSTRUCTION IMPROVEMENTS AFFECTING THE EXISTING CONDITION, PERFORMANCE AND LIFE CYCLE OF VILLAGE STREETS, ALLEYS, OR RIGHTS-OF-WAY SHALL BE RESTORED TO THE REQUIREMENTS AND SATISFACTION OF THE CITY OF SIDNEY'S ENGINEERING DEPARTMENT. ALL CITY INFRASTRUCTURE SHALL BE ADEQUATELY RESTORED ACCORDING TO APPLICABLE STANDARDS AND DETAILS.

**D.** ALL NEW SUBDIVISIONS AND DEVELOPMENTS SHALL BE PROVIDED WITH PUBLIC SIDEWALKS ON BOTH SIDES OF STREETS EXCEPT INDUSTRIAL ZONED AREAS THAT REQUIRE SIDEWALKS ON THOROUGHFARES, COLLECTORS, AND ANY OTHER STREETS AS DIRECTED BY THE PLANNING COMMISSION IN ACCORDANCE WITH CITY ORDINANCE 1161.05(3)(C).

**E.** CURB CUTS FOR ALL NEW AND RECONSTRUCTED DRIVEWAYS SHALL CONFORM TO VILLAGE STANDARDS. ALL NEW DRIVEWAY APPROACHES SHALL BE CONSTRUCTED OF CONCRETE AND SUBJECT TO ALL VILLAGE REQUIREMENTS INCLUDING A MONETARY FEE (AS SET BY CODIFIED ORDINANCES, CHAPTER 902.02) PAYABLE TO THE VILLAGE AND THE ACCESS MANAGEMENT POLICY..

**F.** NO VILLAGE STREET OR ALLEY SHALL BE CLOSED WITHOUT PERMISSION FROM THE ENGINEERING DEPARTMENT. APPROVAL MUST BE OBTAINED 48 HOURS IN ADVANCE OF CLOSURE FOR A NON-EMERGENCY SITUATION. ADVANCED PUBLIC NOTIFICATION AND PUBLISHING SHALL BE A MINIMUM OF 24 HOURS PRIOR TO CLOSURE.

## PAVEMENT REPLACEMENT

**A.** IMMEDIATELY AFTER PLACEMENT OF BACKFILL IN EXISTING STREETS, A TEMPORARY PAVEMENT SHALL BE INSTALLED AND THE STREET OPENED. TEMPORARY PAVEMENT SHALL CONSIST OF 8" OF COMPACTED ODOT SPECIFICATION 411 OR 307. THE SURFACE SHALL BE MAINTAINED FLUSH WITH THE EXISTING STREET.

**B.** ANY SETTLEMENT OF A TRENCH CAUSING A DEPRESSION SHALL BE REFILLED AS REQUIRED BY THE VILLAGE AT THE CONTRACTOR'S EXPENSE. THIS PROVISION APPLIED FOR A ONE YEAR PERIOD AFTER WORK IS ACCEPTED BY THE VILLAGE.

**C.** ALL TEMPORARY PAVEMENT AND SIDEWALK SHALL BE MAINTAINED BY THE CONTRACTOR OR DEVELOPER AT HIS OWN EXPENSE IN A SUITABLE AND SAFE CONDITION FOR TRAFFIC UNTIL PERMANENT REPLACEMENT IS MADE OR THE PROJECT IS FINALLY ACCEPTED BY THE VILLAGE.

## TRAFFIC CONTROL

**A.** THE CONTRACTOR SHALL MAINTAIN TRAFFIC CONTROL AT ALL TIMES WITH THE PROPER DEVICES AS PER THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THESE CONTROL DEVICES SHALL BE IN PLACE PRIOR TO ANY WORK COMMENCING. CONTRACTOR WILL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL ITEMS.

**B.** TRAFFIC SHALL BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE VILLAGE.

## CURB STAKING AND ROADWAY

**A.** LINE AND GRADE EVERY 25' ON A CONVENIENT OFFSET WITH TACKED HUBS.

## PAVEMENT (ASPHALT)

**A.** THE CONTRACTOR SHALL PROVIDE THE VILLAGE WITH A COPY OF THE NORMAL (MEDIUM TRAFFIC) ODOT 404 JOB MIX FORMULA FOR EACH PLANT THAT PROVIDES HOT MIXED ASPHALT TO THIS PROJECT. ALL MIXES SHALL FOLLOW ODOT JOB MIX FORMULA WITH THE EXCEPTION THAT THE BITUMEN CONTENT SHALL BE 0.2% HIGHER. SECTION 401.02 COMPOSITION OF THE CURRENT ODOT SPECIFICATIONS SHALL BE USED FOR ACCEPTANCE BASED ON THE INCREASED BITUMEN. A 448 OR 446 JOB MIX FORMULA WILL NOT BE ACCEPTABLE. RECYCLED ASPHALT SHALL NOT EXCEED 15% OF ANY 402 MIX PRODUCED. NO RECYCLED ASPHALT MAY BE USED IN THE ITEM 404 SURFACE COURSE.

**B.** THREE-WHEEL STEEL ROLLER OR EQUIVALENT SHALL BE USED FOR INITIAL BREAKDOWN ON ALL PROJECTS. THIS PROVISION SHALL BE FOR PLACING ASPHALT GREATER THAN 1-1/2" THICKNESS.

**C.** ALL WORK SHALL ADHERE TO ODOT'S LATEST REVISIONS AND TO THE VILLAGE SPECIFICATIONS. WHICHEVER IS MORE STRINGENT SHALL PREVAIL UNLESS OTHERWISE APPROVED.

**D.** PATCHED AREAS SHALL BE SEALED ON THE PERIMETER OF THE PATCH WITH ASPHALT CEMENT.

**E.** ALL UTILITY ADJUSTMENTS --- MANHOLE, WATER VALVES, ETC., --- SHALL BE RAISED TO FINISHED GRADE BEFORE THE FINAL ASPHALT COURSE IS LAID. NO CONCRETE TO SURFACE, UNLESS OTHERWISE APPROVED.

**F.** ASPHALT CEMENT SHALL BE USED NEXT TO THE LIP OF GUTTER PRIOR TO THE FINAL ASPHALT LIFT BEING PLACED. (SS-1 TACK OR PG64-22 SEAL.)

**G.** TACK COAT SHALL BE APPLIED PRIOR TO THE PLACEMENT OF THE FINAL LIFT OF ASPHALT IF THE EXISTING ASPHALT LIFT IS DIRTY OR AFTER TEN DAYS UNLESS OTHERWISE APPROVED. TEMPERATURE MUST BE 50°F OR HIGHER.

**H.** NO ASPHALT SHALL BE PLACED OVER EXCAVATED TRENCHES UNLESS TRENCHES HAVE BEEN COMPACTED AS PER VILLAGE CONSTRUCTION STANDARDS & DRAWINGS PAGE 500-2.

**I.** NO ASPHALT SHALL BE LAID UNLESS THE VILLAGE IS GIVEN PRIOR NOTICE AND THE AMBIENT TEMPERATURE IS 50° F OR GREATER. ALSO NO ASPHALT WORK SHALL BE PERFORMED IN INCLEMENT WEATHER WITHOUT APPROVAL FROM THE PUBLIC WORKS DIRECTOR.

**J.** FINAL LIFT OF ASPHALT SHALL BE FINISHED TO 1/4" ABOVE THE LIP OF GUTTER.

**K.** TEMPERATURES FOR BREAKDOWN ROLLING SHALL BE 260°F PLUS 15°F AND FOR FINAL ROLLING 175°F PLUS 15°F.

**L.** ASPHALT CEMENT SHALL BE USED ON ALL JOINTS AND FEATHERED SURFACES PRIOR TO PLACEMENT OF THE NEXT COURSE OF ASPHALT TO THE ABUTTING JOINT, UNLESS OTHERWISE APPROVED.

**M.** 325°F IS THE MAXIMUM TEMPERATURE ASPHALT MATERIAL IS TO BE MIXED.

**N.** ALL EDGES TO BE TRIMMED BACK AND SAW CUT TO SOLID MATERIAL AND BE STRAIGHT AND NEAT AS PER THE CITY'S INSTRUCTIONS.

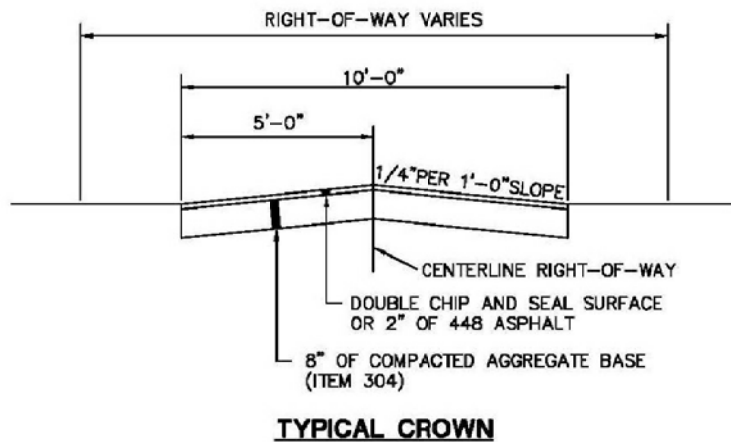
**O.** AREAS TO BE PAVED, OVERLAID OR PATCHED SHALL BE CLEAN AND FREE OF DEBRIS.



# MISCELLANEOUS ROADWAY NOTES

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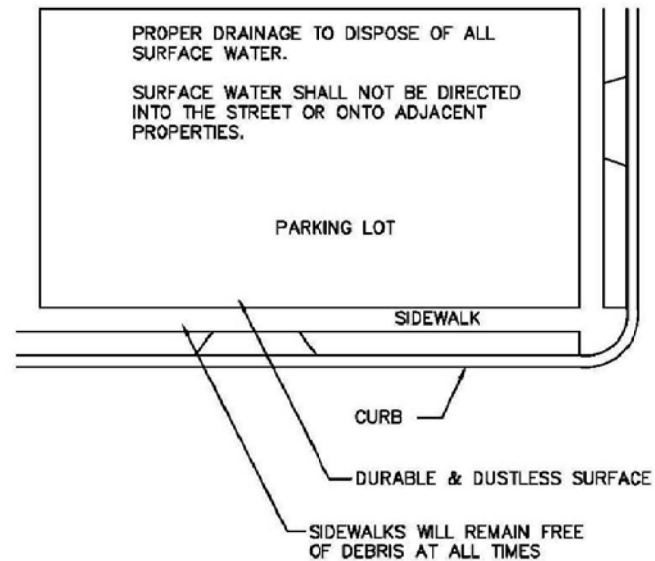
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#### **TYPICAL ALLEY CONSTRUCTION**

- A. MINIMUM STANDARD (UNLESS OTHERWISE APPROVED).
- B. FOR RENOVATION OF EXISTING ALLEYS ONLY. NO NEW ALLEYS WILL BE APPROVED WITHIN THE VILLAGE.

ADJACENT PARKING AREAS SHALL BE CONNECTED TO LIMIT THE NUMBER OF ACCESS DRIVES TO THE STREET.



#### **PARKING LOT DETAIL**

THE FOLLOWING ARE ACCEPTED LOT SURFACES (UNLESS OTHERWISE APPROVED).

- A. DOUBLE CHIP AND SEAL, WITH APPROVAL.
- B. ASPHALT CONCRETE ITEM 448.
- C. CONCRETE.



# ALLEY AND PARKING LOT DETAIL

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## NOTES

**A.** A PUBLIC RIGHT-OF-WAY OPENING PERMIT TO PERFORM ANY WORK ON OR WITHIN A PUBLIC RIGHT-OF-WAY, (STREET, ALLEY, ETC.) IS REQUIRED. A PERMIT IS REQUIRED FOR ANY ITEMS INCLUDING BUT NOT LIMITED TO TUNNEL, SIDEWALK, DRIVEWAY, OPENING OR EXCAVATION UNDER OR IN THE RIGHT-OF-WAY OF PUBLIC GROUNDS. IN ADDITION, A MONETARY FEE \_\_\_\_\_

PAYABLE TO THE VILLAGE IS REQUIRED FOR ALL CURB CUTS AND DRIVEWAYS.

**B.** PERMIT FORMS ARE AVAILABLE FROM THE VILLAGE \_\_\_\_\_. THE PERMIT FORM WILL BE COMPLETED BY THE PERSON OR FIRM PLANNING THE WORK WITHIN THE RIGHT-OF-WAY. ALL APPROVALS MUST BE OBTAINED BEFORE ANY WORK IS STARTED. 72 WORKING HOUR LEAD TIME IS RECOMMENDED.

**C.** THE APPLICANT SHALL HAVE SUFFICIENT BARRICADES, WARNING SIGNS, AND LIGHTS DURING THE ENTIRE PERIOD THAT THE WORK IS BEING PERFORMED AND SHALL ADHERE TO APPLICABLE SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

**D.** A PERMIT FEE OF \$50 PLUS MATERIAL AND LABOR COST FOR THE REPAIR WILL BE CHARGED OF EACH APPLICANT THAT FAILS TO PROPERLY REPAIR A STREET CUT TO VILLAGE STANDARDS. THIS CHARGE WILL BE ENFORCED BY THE VILLAGE STREET DEPARTMENT.

**E.** THE EXISTING PAVEMENT SHALL BE NEATLY SAWCUT PRIOR TO EXCAVATION. ALL EXCAVATED MATERIAL SHALL BE REMOVED FROM THE JOB SITE. THE APPLICANT IS RESPONSIBLE FOR ALL PAVEMENT DAMAGED OUTSIDE THE TRENCH AREA.

**F.** ALL EXCAVATIONS OR TRENCH EDGES UNDER OR WITHIN 5' OF PROPOSED OR EXISTING PAVEMENT, CURB, DRIVEWAYS, ALLEYS, STONE AREAS OR WALKS SHALL EITHER BE BACKFILLED WITH LOW STRENGTH MORTAR BACKFILL ODOT ITEM 613, TYPE 1 ONLY OR BACKFILLED WITH ODOT 603 TYPE 1 OR TYPE 2 GRANULAR MATERIAL, COMPACTED IN 6" LAYERS. A DENSITY TEST OF 98% OF ASTM D698 STANDARD PROCTOR CURVE MAY BE REQUIRED TO BE PERFORMED BY A COMMERCIAL TESTING LAB SATISFACTORY TO THE VILLAGE.

**G.** ALL EXCAVATION OR TRENCH EDGES NOT UNDER OR WITHIN 5' OF PROPOSED OR EXISTING PAVEMENT, CURB, DRIVEWAYS, ALLEYS, STONE AREAS OR WALKS CAN BE COMPACTED WITH EXISTING NATIVE MATERIAL IN 12" MAXIMUM LIFTS OR AS APPROVED BY THE VILLAGE.

**H.** SIDEWALKS, CURBS AND DRIVEWAYS SHALL BE REPLACED IN ACCORDANCE WITH NEW CONSTRUCTION IN COMPLIANCE WITH THESE STANDARDS. CONTRACTOR SHALL NOTIFY THE VILLAGE SERVICE DEPARTMENT FOR INSPECTION AFTER FORMS ARE SET AND BEFORE CONCRETE IS ORDERED.

**I.** GRASSED AREAS SHOULD BE BROUGHT TO LEVEL WITH A MINIMUM OF 3 INCHES OF TOP SOIL THEN SEEDED OR SODDED.

**J.** ALL DISTURBED AREAS MUST BE RETURNED TO AS GOOD OR BETTER CONDITION. ALL REPAIRS MUST MEET VILLAGE SPECIFICATIONS. THE VILLAGE MUST INSPECT AND APPROVE ALL REPAIRS.

**K.** COLD PATCH SHALL BE PLACED TO 1-1/2"+ THICKNESS OVER BACKFILLED TRENCH WITHIN ONE WORKING DAY AFTER THE BACKFILL HAS BEEN COMPACTED, IF THE ASPHALT PAVEMENT ISN'T PLACED IMMEDIATELY.



# PUBLIC RIGHT-OF-WAY OPENING AND EXCAVATION

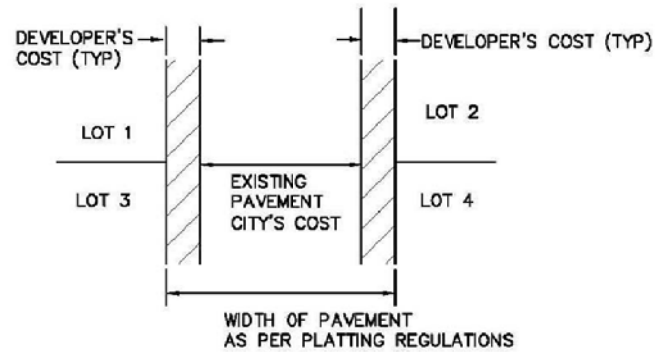
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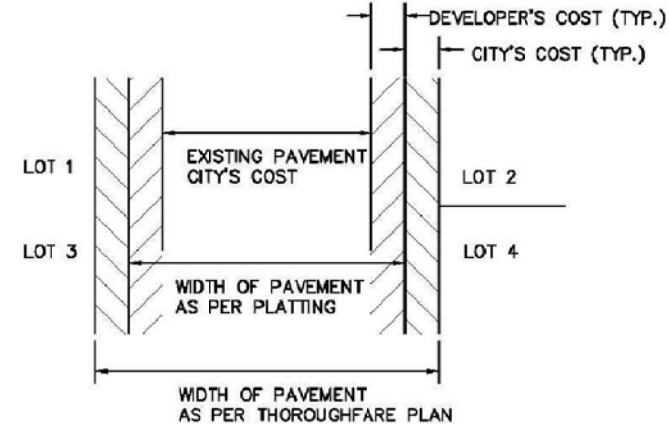


### EXAMPLE 'A'



### STREET IMPROVEMENTS FROM EXISTING STREET WIDTH TO PLATTING REGULATION WIDTH

### EXAMPLE 'B'



### STREET IMPROVEMENTS FROM EXISTING STREET WIDTH TO THOROUGHFARE PLAN WIDTH

### NOTES

- A. IF BOTH SIDES OF A STREET ARE INCLUDED IN THE SUBDIVISION, THE DEVELOPER PAYS THE TOTAL COST FOR ADDITIONAL WIDTH OF EXCAVATION, PAVEMENT, CURB AND SIDEWALK INCLUDING COST TO BRING THE STORM SEWER SYSTEM UP TO STANDARDS.
- B. IF ONE SIDE OF THE SUBDIVISION ABUTS AN EXISTING STREET, THE DEVELOPER SHALL PAY FOR THE TOTAL COST OF ONE SIDE FOR ADDITIONAL WIDTH OF EXCAVATION, PAVEMENT, CURB AND SIDEWALK INCLUDING COST TO BRING THE STORM SEWER SYSTEM UP TO STANDARDS.
- C. THE VILLAGE PAYS CONSTRUCTION COST ON EXISTING STREET WIDTH AND ANY OVERSIZING TO MEET THOROUGHFARE PLAN.

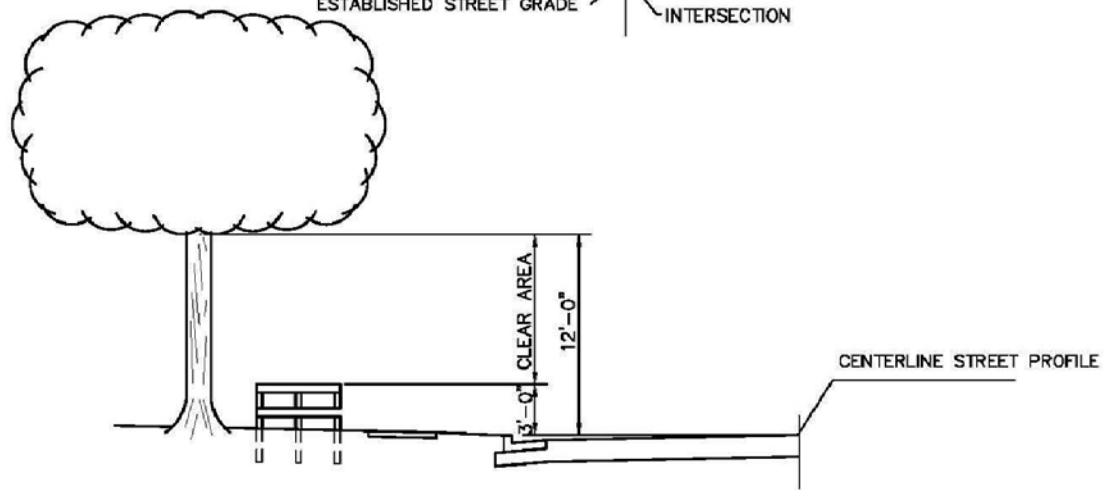
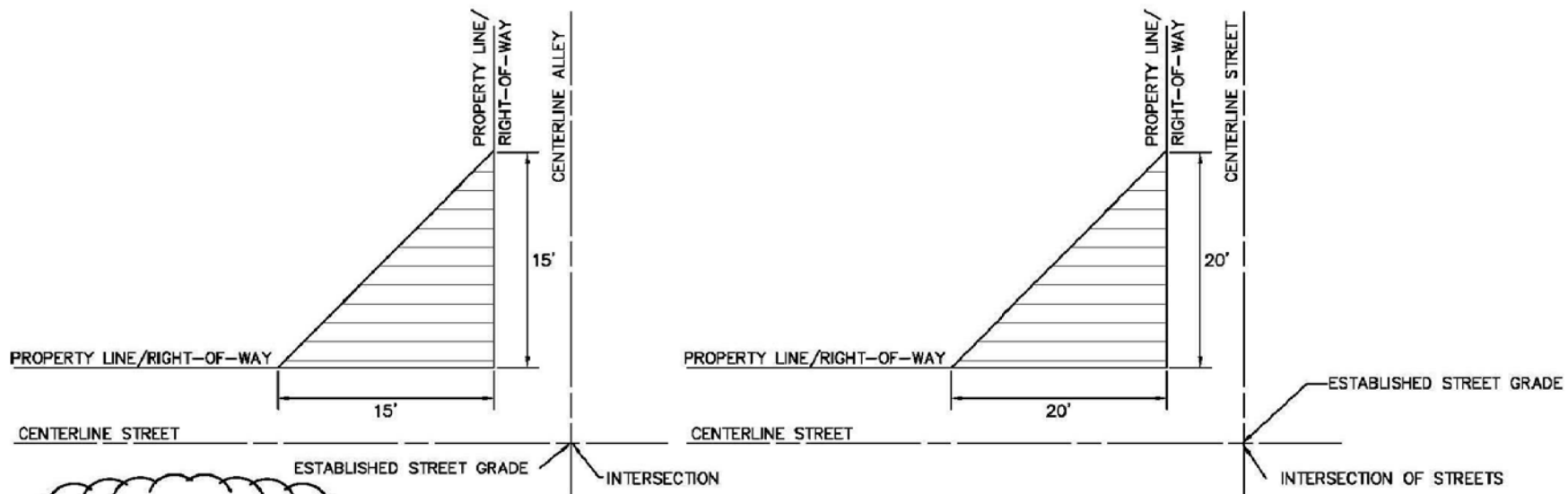


# STREET IMPROVEMENT CONDITIONS

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**VISION CLEARANCE EXHIBIT**

**NOTES**

THERE SHALL BE NOTHING ABOVE 3' OR BELOW 12' OF THE ESTABLISHED STREET GRADE IN THE TRIANGULAR SHADED AREA.

NO STREET TREE SHALL BE PLANTED CLOSER THAN 25 FEET TO ANY STREET CORNER, MEASURED FROM THE POINT OF THE NEAREST INTERSECTING CURBS OR CURB LINES. NO TREE SHALL BE PLANTED CLOSER THAN 10 FEET TO ANY FIRE HYDRANT.

THIS STANDARD ONLY APPLIES TO INDUSTRIAL/COMMERCIAL, COLLECTOR, AND LOCAL STREETS. ARTERIALS SHALL FOLLOW THE SIGHT DISTANCE GUIDELINES SET FORTH IN THE "OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".



# VISION CLEARANCE ON CORNER LOTS

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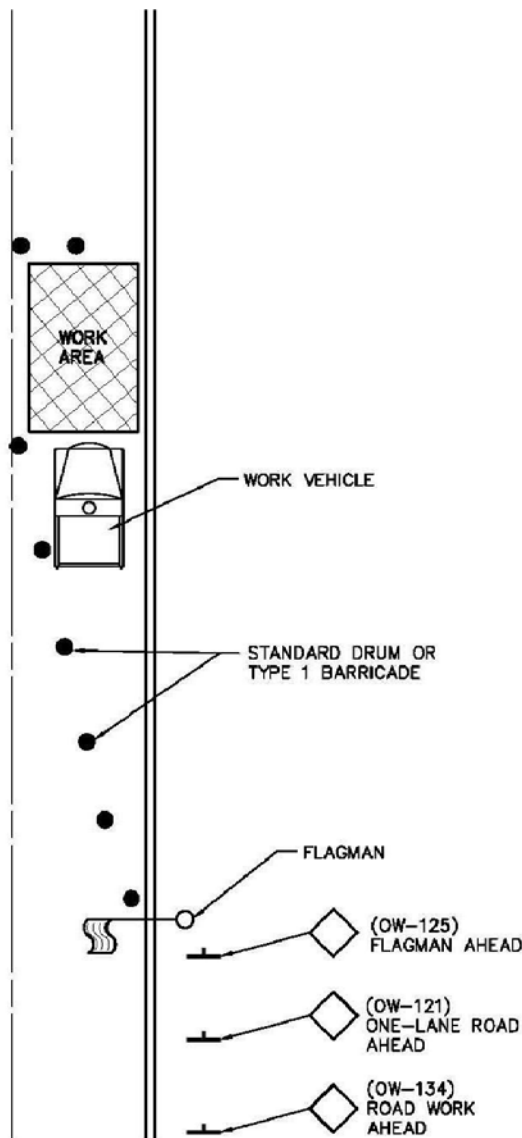
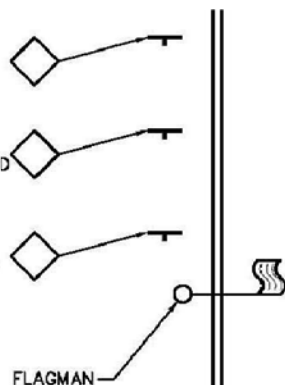
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(OW-134)  
ROAD WORK  
AHEAD

(OW-121)  
ONE-LANE ROAD  
AHEAD

(OW-125)  
FLAGMAN AHEAD



## NOTES

**A.** THE POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED 24 HOURS IN ADVANCE OF ANY CONSTRUCTION. NO STREET SHALL BE CLOSED WITHOUT THE APPROVAL OF THE VILLAGE.

**B.** IF THE WORK IS TO COVER THE ENTIRE WIDTH OF THE STREET, ONE HALF OF THE STREET SHALL BE MAINTAINED FOR TRAFFIC WHILE ONE HALF OF THE STREET IS REPAIRED.

**C.** BARRICADE DISTANCE AND SEPARATION OF WARNING TO BE SPACED AS PER JOB SITE ACCORDING TO THE VILLAGE.

**D.** IF BARRICADES ARE TO BE LEFT UP OVERNIGHT, WARNING LIGHTS (FLASHERS) ARE TO BE USED.

**E.** ALL STREET CONTROL DEVICES APPLICABLE TO DIFFERENT WIDTH STREETS, TYPE OF CONSTRUCTION, ETC., SHALL CONFORM TO THE LATEST REVISION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, UNLESS OTHERWISE APPROVED BY THE CITY AND SHALL BE IN PLACE AND PROPERLY DISPLAYED PRIOR TO THE COMMENCEMENT OF ANY WORK.

**F.** NO WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL COMMENCE WITHOUT APPROVAL FROM THE SERVICE DEPARTMENT WITH A RIGHT-OF-WAY OPENING PERMIT.

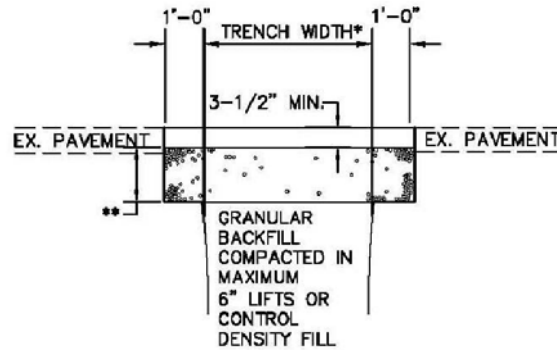


# TRAFFIC CONTROL DEVICES STATIONARY OPERATIONS IN ONE LANE

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\* MIN. 18" WIDTH IF ASPHALT BACKFILL IS USED IN THE REPAIR.

\*\* CONCRETE OR 301 ASPHALT AGGREGATE BASE TO MATCH EXISTING CONDITION.

### TYPICAL PAVEMENT RESTORATION DETAIL

### TYPICAL PAVEMENT RESTORATION NOTES

#### MINIMUM GRAVEL PAVEMENT REPLACEMENT

2" OF ODOT #67 ON

12" OF ODOT ITEM 304, IN LIFTS OF 3" MAXIMUM

#### MINIMUM ASPHALT PAVEMENT REPLACEMENT

PERMANENT PAVEMENT REPLACEMENT SHALL MATCH EXISTING PAVEMENT THICKNESS, BUT MUST BE A MINIMUM OF 3-1/2 INCHES, WHICHEVER IS GREATER. (MINIMUM PAVEMENT COMPOSITION SEE PAGE 300-2 UTILIZING APPROPRIATE STREET CLASSIFICATION).

SOIL BORINGS SHALL BE CAPPED WITH A MINIMUM OF 9" OF ODOT CLASS C CONCRETE.

#### EXISTING PAVEMENT

PAVEMENT SHALL BE CUT IN NEAT, STRAIGHT LINES TO THE FULL DEPTH OF THE EXISTING PAVEMENT, OR AS REQUIRED BY THE VILLAGE ENGINEER.

#### PAVEMENT RESTORATION FOR PIPE INSTALLATIONS

WHERE PROPOSED UTILITY LINE IMPROVEMENTS DISTURB THE EXISTING PAVEMENT, THE CONTRACTOR SHALL RESTORE THE PAVEMENT SURFACE BY PLACING A 4" THICKNESS OF ITEM 301 OVER A 4" THICKNESS OF ITEM 304 WITHIN THE DISTURBED LIMITS. PAYMENT FOR PAVEMENT RESTORATION AND RESTORATION OF DISTURBED FACILITIES AND SURFACES RESULTING FROM CONSTRUCTION OF THE UTILITY SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT UTILITY LINE ITEMS.



# TYPICAL PAVEMENT RESTORATION DETAIL

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EXAMPLE: Settlement / Vertical Offset

(1) Curb and/or gutter having differential settlement or heaving at joints that create a vertical offset of 2 inches or more at any point along a joint,...or multiple joint gaps greater than 3 inches



EXAMPLE: Extreme Slope

(2) Curb and/or gutter having a slope of more than 2 inches per foot in any direction from a road or natural grade that creates a discontinuation in the curb or gutter.



EXAMPLE: Large Holes in Surface

(3) Curb and/or gutter having holes or depressions in the surface that are in excess of 1 foot by 1 foot by 2 inches deep.



EXAMPLE: Deteriorated

(4) The surface of a curb and/or gutter having 50% or greater of its surface continuously spalled or deteriorated within a 10 ft poured section.



EXAMPLE: Exposed aggregate

(5) Curb and/or gutter sections that have crushed stone or gouges that continuously expose aggregate for 50% or more within a 10 ft poured section.



EXAMPLE: Asphalt repairs

(6) Curb and/or gutter sections that have been previously patched with a bituminous material or materials other than concrete for 50% or more within a 10 ft poured section. (This does not include a street or section of a street originally installed or reconstructed with asphalt curbing and/or gutter.)



# REPLACEMENT CRITERIA FOR CURB AND GUTTER

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EXAMPLE: Holding Water

(7) Curb and/or gutter sections that have settled, causing the surface to hold large amounts of water after a rainfall (of 2 inches or more deep over a 20 ft section) or that are tilted or settled so as to restrict storm water drainage within a 10 ft. poured section.



EXAMPLE: Grass/Weed Growth

(8) Curb and/or gutter sections that have missing concrete, been removed or deteriorated so as to have soils and/or grass/weeds growing in its place at 50% or more within a 20 foot section.



EXAMPLE: Low Curb Reveal

(9) Curb and/or gutter sections that have settled or have been built up with asphalt over time so that less than 1 inch of curb is revealed above the pavement and asphalt grinding is not a solution to improve the reveal.



EXAMPLE: Sidewalk to Pavement

(10) Curb and Gutter sections that are combined with sidewalk will not be rebuilt or repaired if the sidewalk is in good repair, even if the curb reveal is low. Other curb deficiencies, while part of the sidewalk, will be repaired only if RECOMMENDED BY THE VILLAGE ENGINEER.



EXAMPLE: Gutter at Driveway

(11) Curbs adjacent to and in front of driveways are not repaired or REPLACED BY THE VILLAGE AND ARE THE RESPONSIBILITY OF THE OWNER AS PART OF THEIR DRIVEWAY PERMIT.

#### **CURB AND GUTTER REPLACEMENT NOTES:**

(12) If over 75% of the property owner's curb and/or gutter meets the replacement criteria and the remaining is less than 20 ft in length and has some damage, the remaining curb and gutter may be replaced as well.

(13) The VILLAGE ENGINEER AT HIS/HER DISCRETION, MAY require that additional curb and gutter be replaced if: a) continued and immediate structural damage is likely to occur without replacement; or b) a section of curb and gutter is determined to be hazardous.

(14) Curb and gutter "spot" replacements, as selected according to VILLAGE REPLACEMENT STANDARDS WILL BE A subset of the annual VILLAGE STREET RESURFACING programs. No other curb and gutter improvements will be planned by the VILLAGE, EXCEPT IN EMERGENCY situations or during a project re-builds.



# REPLACEMENT CRITERIA FOR CURB AND GUTTER

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