

# NEW MEXICO STEAM LOCOMOTIVE

AND RAILROAD HISTORICAL SOCIETY  
P. O. BOX 27270, ALBUQUERQUE, NM 87125-7270 TEL (505)332-2926

## WHAT'S HAPPENING ON SITE? Cold, High Wind, Blowing Dust? Such Elements Do Not Deter The 2926 Crew

Even though late winter weather can make things uncomfortable at times, the work level at the restoration site has actually increased.

In the past few weeks, activity includes:

- ◆ Completion of ultrasound work on the boiler.
- ◆ Cleaning and repair of a numerous parts and appliances is proceeding apace.
- ◆ Thanks to Lawrence Harmon's determined efforts, both forklifts and the car mover have experienced either complete rebuilds or at least serious maintenance.
- ◆ With Bob DeGroft and Dave Traudt leading the pack, a complete accounting, tagging, and photographing where necessary of removed and stored parts is underway.
- ◆ And last, but far from least, there is now an inspection pit on site.

More about the pit later, (see below and in inserted pictorial) Suffice to say that the 2926 crew has never been busier. Busy enough, in fact that CMO DeGroft established an every day work schedule. That schedule will continue for at least a few more weeks.

## INSPECTION PIT

If 2926 were in the backshops, this pit would have been unnecessary. On the other hand, due to a herculean effort by several members, NMSL&RHS has a much more functional pit than the narrow, shallow ones in the shops.

The new pit will allow easy access to the bottom side of the locomotive, tender, and any other rolling stock, for virtually any purpose.

Building a pit made a point. NMSL&RHS members and friends have a lot of talent.

The pit was to be designed and built by a local firm experienced in pit construction. Due to a heavy workload and unforeseen problems, the contractor cancelled at the last moment.

Up stepped members Don McCarnack, a design engineer, and Randy McIntire, a general contractor. Don designed the pit structure and Randy brought in a contractor friend to do the digging and concrete work.

Rick and Gail Kirby, Jim Hills, Dave Traudt, Frank Gerstle, Bob DeGroft, and a number of other members pitched in to do the welding, rail replacement, and other finish tasks. The pit was finished on schedule.

*(See Insert For Detailed Pictorial)*

## RESTORATION PROGRESS NMSL&RHS Mission: Miracle On Eighth Street

### Introduction

In 1995, Ed Bukove had a mission. He hoped to rescue AT&SF 2926 from a rusty demise on Coronado Park. His father had once operated the huge locomotive. Ed wanted to see it run again. He met skeptical responses like:

**"It will take a miracle to make that thing run again.**

**"Where will you find the skills necessary to do such a restoration?"**

**"Where will you find that kind of money?"**

Ignoring the skeptics, Ed gathered a few rail fans and moved on. NMSL&RHS was thus created, eventually acquiring 2926 in late 1999. Some

JUNE 2000



*Five Years After Founder Ed Bukove Held The First NMSL&RHS Meeting, 2926 Is Ready To Leave The Park She Rested For 44 Years.*

months later, thanks to Messer Construction Co., 2926 left the park for a siding near Menaul Blvd. There she rested next to the main line for almost two years.

Society membership grew. But by May 2002, NMSL&RHS was still operating on a shoestring. Then BNSF stepped up and moved 2926 to its current site at 1833 8th St NW. Skills needed for many restoration tasks were found within the membership. Other tasks required few skills—just hard labor under dirty uncomfortable conditions. Eager volunteers stepped forward to address such tasks.

**But what about money for equipment, materials, parts, and supplies?**

**What about funding for skills not available within the Society?**

Financial support is usually a primary concern in such grand projects. This case was no different. NMSL&RHS, an all-volunteer organization, had purchased 2926 from the City of Albuquerque for \$1.00. At the turn of the millennium the entire monetary resources of the organization was far closer to that \$1.00 purchase price than the enormous estimated cost of restoration.

Since then, the Society has made significant progress. But continued support, especially financial support, is needed. The following financial report (Back Page) by President Mike Hartshorne provides both pictorial and tabular views of progress, current asset value, and future financial needs of the Society's locomotive restoration program.

JUNE 2000 TO MAY 2002



*2926 Parked On A Rail Siding near Menaul Blvd and First St.*

*(Continued on Page 4)*



This newsletter is published quarterly by the New Mexico Steam Locomotive and Railroad Historical Society, a Non-Profit Corporation.

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## Board Of Directors Change

Bob Scott, NMSL&RHS Vice President, and 'go-to-guy' in Santa Fe has left us—at least for a while. He was called out of retirement for a shipbuilding project that will keep him fully occupied and on the move.

Until a few years ago, Bob, highly regarded in ship design, was President of Gibbs & Cox, Inc, a large international shipbuilding company. Upon retirement, he moved to Santa Fe and joined NMSL&RHS.

Bob's ability to develop detailed project plans, and to communicate with governmental organizations will be sorely missed.

We wish him the best of luck. Hopefully, his return to shipbuilding will still allow time for him to stay in touch and visit us (and 2926) on occasion.

Bob's term on the NMSL&RHS Board of Directors will be completed by Ed Strebe. The position of Vice President will be determined at the next Board meeting.

\* \* \* \* \*

## Board Election 2009

Pursuant to Article VI of the By Laws of the New Mexico Steam Locomotive and Railroad Historical Society, must hold an annual election of Board of Directors. This year (2009), the term of office for BoD members Marlin Allison and Ed Strebe will expire in July.

Any member in good standing, including the incumbents, who meet the requirements as set forth in Article VI of the NMSL&RHS By Laws is eligible to seek election to the BoD.

Nomination of candidates for BoD open positions may be submitted to any BoD member, prior to 1 May, 2009.

Once nominations are received, a blank ballot with all candidates listed in alphabetical order will be posted on the NMSL&RHS web site. Members with internet access should print out the ballot, indicate their choice and return it to the NMSL&RHS Secretary.

Members who receive the newsletter in hardcopy format will receive a ballot in the mail no later than 15, May, 2009.

For votes to be recorded, ballots must be returned to the Secretary or mailed to the NMSL&RHS, P O Box 27270, Albuquerque NM 87125-2720, on or before 1 July, 2009.

The ballot will contain more detailed instructions regarding completion and mailing.

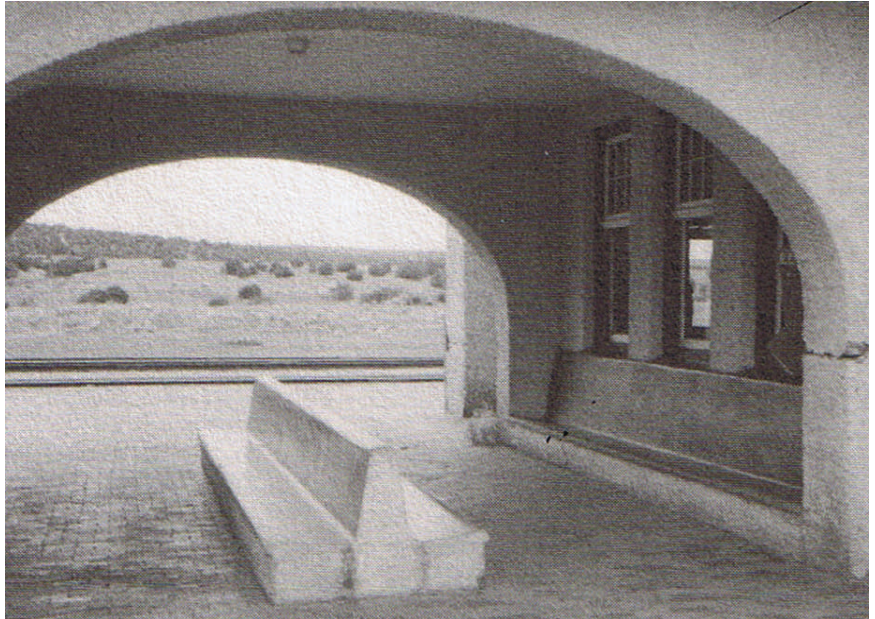
# BANGING ON A 2900

ROBERT "PETE" ADAIR

It was a sunny day September, 1947 when the Santa Fe Chief rolled to a stop at Lamy, New Mexico. The Chief had stopped to let five year old Pete, his three year old sister and his nine month pregnant mom off at the isolated station miles from civilization.

They had come from Washington's Union station on an ATSF through passenger car on the B&O to Chicago. The ATSF car was parked in the Chicago yard for the night. The traveling Adair's stayed on the car for the night.

The next morning, it was westward-ho on the Atchison, Topeka and Santa Fe. It was a big adventure for young Pete. He had visions of a trip to the beach. After all, when one made a trip from home in Arlington, VA it was often to the beach. Pete was in for a surprise.



*Lots of beach, but where is the water? This view, looking through a breezeway, across the tracks and into the wide open high desert is what young Pete might have seen.*

Some weeks earlier, Pete's dad had started in a job with the Security Department of the newly created Atomic Energy Commission at Los Alamos, New Mexico. He was to meet his family when the Chief arrived at Lamy.



*The AT&SF station at Lamy, NM. The breezeway at the near end of the building contains the outdoor waiting room.*

The conductor was concerned about the certainty that dad would be there. If no one was there to meet them, the conductor felt that he could not abandon an obviously pregnant mom and two kids in lonesome Lamy. He told Pete's mom he would hold the train until he was assured that someone was waiting to pick them up. If there was no one, they were to get back on the train and ride to Albuquerque. At least there were people and a hospital or two there.

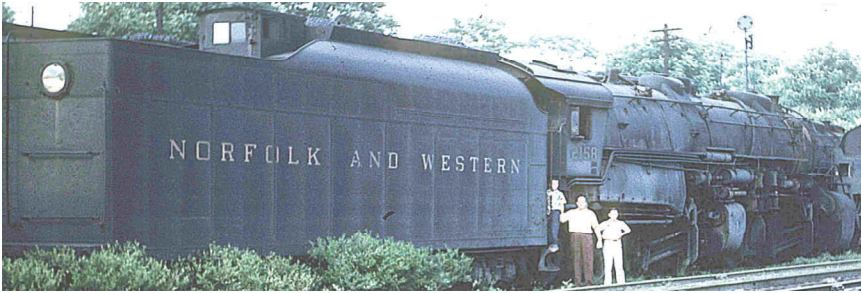
Dad did show up and the Chief proceeded to Albuquerque and points west.

*Continued on page 3, Column 1*



Pete and his newly expanded family spent the next 5 years in Los Alamos. He lived his elementary school years in the same neighborhood where and while Mike Hartshorne was a toddler.

In those secret city days, Los Alamos was referred to as ‘the hill’. For folks on ‘the hill’, entertainment was mostly of a do-it-yourself nature. Pete’s folks were lucky enough to have friends with model train layouts. Pete fell in love with American Flyer and Lionel. He got his own Lionel layout started in 1949 with a K-4. Later, he managed to get a 1950 Christmas upgrade to a yellow Union Pacific Alco Diesel he found in a Los Alamos hardware store (after he figured out the difference between Santa Claus and dear old dad).



*Pete’s interest went beyond scale models to the real thing. Here, on a trip back to Virginia, Pete, Dad and little brother check out a deadlined N&W Y6a compound 2-8-8-2. This class locomotive, built in N&W’s Roanoke shops, was slow but very powerful. Its top speed was only 50mph, but it had much more tractive effort than UP’s Big Boy.*

Pete’s family moved to Albuquerque in 1952. That was to the brand new Hoffmantown home then on the desert edge of civilization (at Wyoming and Meneal). Pete was in the first class at the Sandia High School starting there in 1958 before construction was complete.

Pete joined the Air Force in 1961. After basic training, at Lackland AFB, Texas and Electronic Technical School at Keesler AFB, Mississippi, he was stationed at Biggs Air Force Base in El Paso, home of the 95<sup>th</sup> Bombardment Wing. He kept airborne navigation radars running, for the 431<sup>st</sup> Aerial Refueling Squadron flying KB-50J’s. When that squadron was disbanded in early 1965 he moved to the 95<sup>th</sup> Bomb Wing flying B-52B’s until his discharge.

Out of the Air Force he was back in Albuquerque at the University of New Mexico. He started in Engineering, switched to a business major and graduated in 1971. Getting a degree took quite a while. Like many students of that era, he worked various jobs to finance his education.

In the spring of 1967 he handled HF radio communications during a 3 month weather expedition to the Palmyra Island, in the Line Island group about 1500 miles southwest of Hawaii. He says the coconut crabs there tasted great.

Starting in the summer of 1968 Pete worked a year at Amchitka, Alaska. There, he helped with communications linking to the Nevada Test Site in support of the off-continent high yield nuclear test program. He was a bit reluctant to say what he ate there.

He did say Amchitka food was generally excellent and plentiful—except for the one time that brings to mind an issue he has tried to forget. It happened during a protracted violent Pacific storm. No re-supply aircraft could get in. A supply barge that was on its way with seaborne supplies, broke loose from its ocean tug and disappeared in the ocean for about two weeks. Food supplies ran very low—except for chicken livers. Pete and his buddies at Amchitka were treated each meal to chicken livers prepared in almost every conceivable way. Pete still has problems with such delicacies and chicken livers in general.

Later, back at UNM, Pete got his degree and was ready to launch his career. First, he held an unsatisfactory sales job at Radio Shack. He decided sales was not for him, and got a better job doing technical security work for Wackenhut Services, Inc., a contractor at the Nevada Test Site. He can only say that looking for clandestine listening devices was like “looking for a black cat in a dark room only there is no cat there”. But it paid the bills.

In 1974, Pete experienced changes that determined his future, both at home and work. He married very tolerant wife Michele, and entered the federal service with the Atomic Energy Commission (AEC).

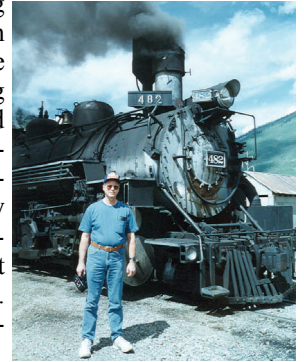
From the marriage came three daughters. One is a microbiologist, one works for a prominent financial firm, and one is a professional mom. The new job established his career path.

The AEC became the Energy Research and Development Administration (ERDA), and subsequently the Department of Energy (DOE). In 1980, he moved his family from Las Vegas, Nevada back to Albuquerque to work for DOE until his 2004 retirement.

One day driving home from work in 1999 he heard on the radio something about some “old guys” who were going to take a locomotive out of a city park. Pete remembered 2926 being put in the park in 1956. As a teenager working summer jobs downtown he had seen it many times.

Dave Anglen, a DOE Safety Officer and an NMSL&RHS member, told Pete more about the organization and its mission. Pete headed straight to Coronado Park and 2026. There, he met Frank Gerstle and Tom Willers. He needed little encouragement from President Gerstle to donate twenty four bucks to Treasurer Willers. He bought a yellow hardhat, and joined the 2926 team—just in time to help roll the grand locomotive out of that park in 2000.

Since then Pete has been in the thick of things every week. He has taken a turn on the Board of Directors, worked as treasurer, helped with yearly audits, pitched in to keep the convenience store running, been at every open house and almost every work session. Most of us know him best with a needle scaler in hand. He has initiated many new members into the wonders of rust pounding with that device.



*Back in New Mexico, Pete’s steam love affair continued with the narrow gauge in New Mexico and Colorado.*



*Working in the 2926 smokebox, Pete helps pull one of the huge locomotive’s flue tubes.*

Where to now? Preferably, Pete will hang with us to complete restoration of 2926. He’ll have earned one of the biggest grins in the crowd when she moves under her own power.

(Continued from Page 1)

**Progress, Status, and Future Needs**

We have come a long way since restoration of 2926 was just dream of Ed Bukove and a few rail fans. We have met one milestone, (restoration of the tender). Financially, we are stronger than we have ever been. Yet, we cannot rest on our accomplishments. Our current financial condition is only a fraction of what we need to complete the restoration of 2926.

A remarkable amount of the value in the 2926 project is donated dollar bills and volunteer hours. The net assets reflect the value of all we own, and include the increasing value of the locomotive, the infrastructure, tools and other tangible assets.

What cannot be calculated is early support. Before 2004, the NMSL&RHS was not as financially mature, and early efforts were poorly documented. Not shown is the remarkable contribution by Messer Inc. of about 130,000 dollars in the move of 2926 from the Coronado Park. Also not shown is the cost of two BNSF engines and crews for the second move.

Lots of \$20 bills, uncounted man hours, numerous “let me help you with that”, and “I thought you guys might be able to use this” contributions were never recorded

When people say locomotive restoration is expensive they are not kidding. NMSL&RHS needs to double and maybe quadruple yearly dollars, hours and in-kind donations over the next two years to finish 2926. For 2009 things are looking better. It may well be the best financial year we have ever had. In a troubled national economy the Society has its brightest prospects for increased support from its members and the community of interested supporters who have come forward with contributions large and small.

The society needs your support now more than ever. All donations are tax deductible. When donating to NMSL&RHS, at least you know that someone will not have “made off” with your money as certain hedge fund operators have done. **Every bit goes into the 2926!** If you need more details, the Society’s audits and IRS 990 forms are available by request to the Board of Directors.

Thanks one and all for generosity told and untold.—Mike Hartshorne, President, NMSL&RHS

**FINANCIAL PROGRESS THRU 2008**

| <b>NEW MEXICO STEAM LOCOMOTIVE &amp; RAILROAD HISTORICAL SOCIETY</b> |                                   |                                |                                    |            |
|--|-----------------------------------|--------------------------------|------------------------------------|------------|
| <b>Financial Picture As Of March 2009</b>                            |                                   |                                |                                    |            |
| YEAR   | CONTRIBUTIONS                     |                                |                                    | Net Assets |
|  | <sup>1</sup> Dollar Contributions | <sup>2</sup> In kind Donations | <sup>3</sup> Volunteer Labor (Hrs) |            |
| 2002   |                                   |                                | 1,915                              |            |
| 2003   |                                   |                                | 3,784                              |            |
| 2004**   | \$19,133                          | 37,518                         | 4,507                              | 286,454    |
| 2005   | 27,234                            | 80,299                         | 5,732                              | 344,972    |
| 2006   | 35,325                            | 66,972                         | 5,426                              | 405,538    |
| 2007   | 40,919                            | 82,896                         | 7,096                              | 491,696    |
| 2008   | 50,772                            | 90,998                         | 8,667                              | 597,424    |
| <b>TOTALS</b>  | <b>\$173,383</b>                  | <b>360,683</b>                 | <b>37,127 Hours</b>                |            |

<sup>1</sup> Includes annual member donations, other cash/check/Credit Card donations, memorabilia sales, grants, etc.  
<sup>2</sup> Includes donated tools and equipment, equipment operation/use, services, materials, paint, supplies, etc.  
<sup>3</sup> Includes labor by NMSL&RHS members and labor volunteered by outside specialists.  
**\*\*** First year of professional audit.

**PROGRESS ON THE GROUND THRU 2008**

(View in 1, 2, & 3. Taken From Same Location Near 8th St And Rail Siding)



Cleaning GSA/BIA Lots & Siding For 2926's New Home



2926 Enters 8th St Gate To New Home



2926 (Center) And Primed Tender (Right Center) At Site.



Tender Restoration Complete, Better Than New, Ready To Roll